



2023 Revised  
Mixed-Use Development Manual  
Penfield, NY

Preface

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# Preface

The Town of Penfield's 2010 Comprehensive Plan called for the creation of a new mixed-use zoning district to provide the community with diverse housing options, promote walkability and connectivity, establish a stronger, more varied economic base, and minimize sprawl-based development patterns.

Additionally, the 2010 Comprehensive Plan recognized the Town's Four Corners and LaSalle's Landing districts as two existing mixed-use areas. The Plan recommended specific areas for new Mixed-Use Development Districts including a section of Fairport Nine Mile Point Road, also referred to as NYS Route 250, in the Sweet's Corners area, and at the former Dolomite quarry on Old Penfield Road known as Manitou Lake. This property has since been sold to a private owner.

On November 1, 2017, the Penfield Town Board adopted Mixed-Use Development District Zoning Ordinance, accepted this document, known as the Mixed-Use Development Manual, and amended the official Town Zoning Map to establish the Mixed-Use Development District.

Since the adoption, several development projects have been reviewed and approved by the Planning Board. Through the various development applications, which varied in size and scope, it became clear that there were areas of the code and Manual that needed to be modified to ensure consistency and clarity in the interpretation of the regulations and provide a clear set of parameters under which mixed-use development projects can be evaluated.

In July 2022, the Town Board established a Moratorium on any new development applications in this district to allow for revisions and modifications to the regulations contained in Section 250-5.12 of the Town Code and the Mixed-Use Development Manual.

## 1.1 About this Manual

This Mixed-Use Development Manual ("Manual") provides information to support the regulations, requirements and standards set forth in the Town of Penfield Mixed-Use Development District Zoning Ordinance. In any instance where there is a discrepancy between the regulations contained in the Town Code and this document, the Town Code regulations shall prevail.

This Manual is organized to provide general information on the concepts of mixed-use development, the background of mixed-use specific to the Town of Penfield, and best-practices guidelines for planning a successful mixed-use development project.

Appendices include application forms and checklists, definitions, maps, examples, sources, and other support material at the time the revised regulations were adopted. These are reference materials only. Applicants are encouraged to contact Penfield Planning and

Engineering Department staff to obtain the required forms and application documents to ensure they have the most up-to-date information.

## 1.2 Target Audience

Town staff and the boards tasked with reviewing Mixed-Use Development applications will work with developers and the community to ensure that mixed-use development projects meet the criteria of the Mixed-Use Development District. This Manual supports the planning, development, and review of mixed-use development projects for all involved parties.

Target audiences and uses of this Manual:

- (1) Town planners and other technical staff may use the Manual as an information source and guide for reviewing, commenting, and making recommendations regarding mixed-use development proposals.
- (2) Decision makers, including members of appointed boards may use the Manual to suggest design or planning options within the context of specific applications.
- (3) Developers may use the Manual for guidance and specific requirements for mixed-use development projects in Penfield.
- (4) Engineers may consult the Manual for considering street and sidewalk designs, site improvements, drainage reports, and utility placement.
- (5) Architects, landscape architects, and designers may use the Manual for best practices in mixed-use design, especially those pertaining to building orientation, entrances, and facades, as well as landscaping, lighting, and signage.
- (6) Real estate and property management professionals for marketing purposes.
- (7) Citizens, including members of neighborhood organizations and nearby property owners, may use the Manual as a source of information to understand potential impacts of mixed-use development with respect to intensity, context, connectivity, and transitions to adjacent uses.
- (8) Property owners within the Mixed-Use District interested in the future potential use and development of their land.

## 1.3 How to use this Manual

The structure of this Manual provides information on general mixed-use concepts followed by Penfield-specific suggestions and guidance. See the Table of Contents for details.

The Manual is intended to be a supporting document to the regulations. Photographic and illustrative examples in this Manual illustrate the ideology of concepts and guidelines; they are not strict directives.

# 1 Executive Summary

## Purpose

The Town of Penfield’s 2010 Comprehensive Plan called for the creation of a new mixed-use zoning district to provide the community with diverse housing options, promote walkability and connectivity, and establish a stronger, more varied economic base. To that end, on November 1, 2017, the Penfield Town Board adopted Mixed-Use Development District Zoning Ordinance, accepted this document, known as the Mixed-Use Development Manual, and amended the official Town Zoning Map to establish the Mixed-Use Development District.

In July 2022, the Penfield Town Board adopted a Moratorium on new development within the existing Mixed-Use District to allow for identified updates and edits to the relevant sections(s) of Town Code and modifications of the Manual needed to modify its function in the development review process.

## Philosophy

Mixed-use development is a planning practice used in urban, suburban and rural areas that combines two or more different types of land uses—such as residential, commercial, services, and entertainment—in a compact setting that emphasizes pedestrian connectivity.

## Expectations

More than five years ago, the Town of Penfield Mixed-Use Development Steering Committee undertook the establishment of the Mixed-Use Development District in Penfield. It was the intent of the committee that implementation of mixed-use development in Penfield would fulfill the spirit and potential of the zoning category. Mixed-Use was not an incremental variation of previously existing zoning ordinances. Projects within the Mixed-Use Development District must consistently meet the standards set forth in the Mixed-Use Development District and embody the design approaches included in this Manual.

Mixed-Use development proposals in Penfield must emphasize pedestrian orientation and connectivity, multi-modal transportation, compact residential development, open space preservation, interconnected streets, and an integration of uses. Proposals for development will feature vertical (in the same building) and/or horizontal (adjacent to each other) mixed-use designs.

Projects should provide a variety of residential uses that are integrated with non-residential space, such as small retail sores, professional offices, municipal or civic uses,

and other compatible non-residential uses. Interconnected streets will disperse traffic, reduce trip length, promote walkability with direct routes, and connect neighborhoods.

Developers are encouraged to present creative ideas to meet the needs of the Penfield community and the spirit of mixed-use development.

## Process

All developers interested in proposing mixed-use developments in the Town of Penfield will need to adhere to the development requirements, design standards and development review process outlined in Town Code Section 250-5.12.

# 2 Introduction to Mixed-Use Development

## 2.1 What is Mixed-Use Development?

### Today

Mixed-use development is a planning practice used in urban, suburban and rural areas that combines two or more different types of land uses in a compact setting that emphasizes pedestrian connectivity, such as, residential, commercial, services, and entertainment.

Land uses must be closely located to promote walking and overall convenient access between destinations. There is an emphasis on pedestrian movement and the use of vehicles and large parking areas is minimized. Common public areas for social interaction and green open spaces are necessary features of mixed-use development.

Mixed-use developments range in scale from large, planned development sites to projects for single structures on smaller parcels. Projects are typically multi-story buildings with commercial/retail at the ground level and offices and residential above. However, they can also be comprised of horizontal mixed-use where varying development types and uses are located adjacent to one another within the same building footprint or in separate buildings located in close proximity.



**Figure 2.1.**  
*New Town Williamsburg is a 365-acre, mixed-use community located in James City County, Virginia.*

### History

Many traditional downtowns and village centers developed before World War II exhibit mixed-use characteristics. Housing above stores was common in village centers and downtown commercial areas. After the war citizens of the middle class grew tired of the



factories and commercial structures that were noisy, unsafe, emitted foul odors, and were indiscriminately located among residential neighborhoods. This disparate juxtaposition drove many families from once thriving urban settings to booming suburbs at the outer rings of metropolitan areas. Municipalities began to enact traditional Euclidian zoning ordinances to help separate land uses.

Early zoning focused on separating different land uses to minimize conflicts, and to protect public health, safety, and welfare at a time when there was little to no oversight of industrial operations. This led to a pattern known as “suburban sprawl” brought on by the housing boom of the post-war era. Many suburban Towns became bedroom communities, cities declined, and resident interaction declined.

Today, planners recognize that suburban sprawl has contributed to a highly unsustainable pattern of development throughout the country. Modern safety and health regulations prevent and/or minimize the past detriments of housing in close proximity to non-residential uses. Over the last two decades or more, mixed-use development has been regarded as a more effective and sustainable alternative to traditional suburban development.

## Advantages of Mixed-Use

Recent planning and land use studies show that, over time, suburbs can be very costly to maintain for municipalities. Today, mixed-use developments are encouraged so that land can be used more efficiently by mixing appropriate non-residential uses with a variety of residential spaces in suburban and urban communities.

Incorporating different land uses within mixed-use developments is considered a “smart growth” tool because it reduces automobile dependency, minimizes sprawl, increases economic development, provides diverse housing options, preserves green space as well as agricultural and natural resources, promotes healthier lifestyles for residents, and a more robust community tax base.



**Figure 2.2.** *The Gulch is an urban mixed-use neighborhood in downtown Nashville, Tennessee encompassing more than 60 acres.*

The following chart shows the average property tax per acre ration based on a study by the Urban Land Institute (ULI) based on 15 cities in the United States.

<b>Average Property Tax based on 15 Cities</b>	
<b>Land Use</b>	<b>Tax Revenue/Acre</b>
Single Family Residential (rural)	\$1.00
Single Family Residential (Urban)	\$3.70
Big Box (Walmart)	\$7.00
Mall or Strip development	\$7.80
2-Story Mixed-Use	\$53.70
3-Story Mixed-Use	\$105.80
6-Story Mixed-Use	\$415.00

*Table 2.1. This chart from the Urban Land Institute (ULI) shows the typical amount of tax revenue generated per acre for each type of land use based on a sample set of 15 different cities from Montana to Florida. Agricultural uses were not included in the study, in part because the way this type of land use is taxed varies significantly from state to state. In many parts of the country, though, agricultural uses can generate as much or more tax revenue as big box or strip development.*

### 2.1.1 Types of Mixed-Use

There are many types of mixed-use development. The following list describes some of the most common applications. The mix of uses described below may not be appropriate for all mixed-use developments. Some projects emphasize residential uses while others focus on commercial applications. The appropriate mix of uses is directed by zoning ordinances and influenced by existing land use of adjacent areas, community needs, and market trends.

(1) Neighborhood Commercial – Convenience goods and services permitted within a residential development such as a convenience store, childcare center, or small restaurant.

(2) Main Street Residential/Commercial – A vertical style often found in historic Town centers featuring two- to three-story street-facing buildings with residential units above street level commercial uses.

(3) Urban Residential/Commercial – A high-density urban development consisting of multi-story buildings that are primarily residential, but include commercial, office, or civic uses on lower floors.

(4) Office/Residential – Combination of single-family residential and multi-family residential uses conjoined with office uses.

(5) Office Convenience – Small retail or service uses permitted within a larger office development.

(6) Retail District Retrofit – Suburban strip plazas retrofitted for a more village-like appearance and mix of uses.

(7) Live/Work – Vertical style that allows residents to operate small businesses or offices on the ground floor of a building in which they reside.

(8) Studio/Light Industrial – Permits residents to operate studios or small workshops within the same building in which they reside to assist in business incubation and reduce living expenses.

(9) Traditional Neighborhood Development – A full range of uses and housing types emulating neighborhoods built before World War II.

(10) Cluster Development/Land Conservation – Emphasizes clustered residential development to concentrate density and preserve adjacent open space, sensitive environmental areas, or valuable agricultural land.



**Figure 2.3.** *Small neighborhood markets located within, or adjacent to, mixed-use developments help meet the needs of residents while also promoting walkability.*



**Figure 2.4 - 2.5.** *These mixed-use developments show the importance of pedestrian connectivity. This development in Grand Rapids, Michigan, has wide sidewalks and attractive street furniture. Brightleaf Square in Durham, North Carolina, is a mixture of commercial and residential uses. Turn-of-the-century tobacco warehouses were resurrected as a shopping center with specialty stores and restaurants.*

## 2.1.2 Mixed-Use is not Multi-Use

Mixed-use developments are different than multi-use developments; the two should not be confused.

Mixed-use emphasizes pedestrian orientation and connectivity, multi-modal transportation, compact residential development, open space preservation, interconnected streets, and integration of uses. Mixed-use developments can be horizontal (adjacent to each other) or vertical (in the same building).



**Figure 2.6.** *These American Craftsman style homes in Libertyville, North Carolina, are an example of the types of layout often used in mixed-use development.*

Mixed-use developments typically provide a variety of residential uses that are integrated with commercial buildings, small retail stores, offices, public open space, civic and other compatible uses. Interconnected streets can disperse traffic, reduce trip length, encourage walkability by providing direct routes, and connect neighborhoods. Further, uses are integrated, closer together, and linked by trails and walkways. Drive-thru operations are not allowed because they emphasize automobile use.

In contrast, multi-use developments are not integrated, there is not an emphasis on walkability, streets are not interconnected, and the community remains automobile dependent.

## 2.2 Ten Principles of Mixed-Use Development

All projects will follow the ten universal principles of mixed-used development:

- (1) A mixture of complementary land uses to create economic and social vitality and encourage the linking of pedestrian and vehicular trips.
- (2) Flexible housing alternatives.
- (3) Areas that are safe, comfortable, and convenient for pedestrians.
- (4) Flexibility in the siting and design to support future changes in the marketplace.
- (5) Walkability within neighborhoods with walkways and trails that encourage pedestrian and bicycle travel.
- (6) Variety of services within walking distance.
- (7) Efficient use of land with compact, clustered development.
- (8) Development that supports public transit, where applicable.

- (9) Open space preservation/creation and reduction of impact on natural resources.
- (10) Transportation planning that reduces vehicular demands.

Developers should clearly demonstrate how their proposals for mixed-use development address these ten principles.

## 2.3 The Built Environment

Design standards for the built environment provide a framework to inform and inspire developers to construct innovative, sustainable, mixed-use projects that meet community goals and objectives.

Four factors that influence design of mixed-use developments are as follows:

- (1) Context - Existing uses, buildings, and sites in the development area.
- (2) Community - Whether urban, rural, or suburban, a mixed-use development should relate to its immediate surrounding area and be consistent in terms of scale, use, and density.
- (3) Integration - Mixed-use can take many different forms based on context and community. Vertical integration stacks different uses sharing the same building one atop another. Horizontal integration places various uses adjacent to one another.
- (4) Density - Development must have defined minimum and maximum density to provide balance between concentrated uses and public open space.

How effectively these factors are addressed will influence the success or failure of a mixed-use development project.



**Figure 2.7.** *The Kendall Town Center in Jacksonville, Florida, exemplifies many of the characteristics of a traditional main street.*

## 2.4 Characteristics of Mixed-Use Design

In general, mixed-use development zoning districts have unique characteristics based on the surrounding geography and codes set forth by the local municipality, but they often share basic concepts and elements that are considered for mixed-use development.

Mixed-use characteristics include:

- (1) A vertical or horizontal mix of residential, commercial, office, and civic uses in each development.
- (2) High residential densities that concentrate housing units in compact/tall structures, including, but not limited to, low-rise, medium-rise, and high-rise housing, multiplex, or universal design concepts.
- (3) Connectivity of pedestrian paths, bikeways, roads, and structures. All should be accessible, attractive, and inviting, and provide direct connections between places.
- (4) Walkability for pedestrians. Walkways/sidewalks should connect primary building entrances to one another along commercial corridors. They should be well-lit with comfort features including benches, and be visually appealing with landscaping and public art.
- (5) A bicycle-friendly environment with established bicycle lanes and street connections that promote the use of bicycles over automobiles.
- (6) Building heights that are influenced by surrounding land uses, site characteristics, existing fire codes, and community character. Mixed-use development districts often allow taller building heights in order to increase density and preserve open space.
- (7) Building setbacks along property boundaries that are large enough to create buffers between adjacent land uses but may feature more limited internal setbacks.
- (8) Public open space for parks, plazas, playgrounds, and community enjoyment. Public open space should be centrally located to encourage social interaction.
- (9) Private open space that is available only to residents located within the district. This can be in the form of decks, enclosed patios or small back yards, or courtyards accessible only to residents.
- (10) Landscape design that integrates aesthetic and functional requirements of a pedestrian-scale mixed-use development with the inherent character of native plant material.
- (11) Discreet parking locations, orientation, and appearance (parking lots, garages, and off-street parking spaces) that support connectivity and walkable neighborhoods.

Specific criteria for Penfield's Mixed-Use Development District are set forth in Section 250-5.12 of the Town Code.



**Figure 2.8.** This cottage housing in Puyallup, Washington, is built around a central public space.



**Figure 2.9.** This vertical mixed-use building in Denver, Colorado, has retail services on the bottom floor and residential on the top floor.

# 3 Mixed-Use Development in the Town of Penfield

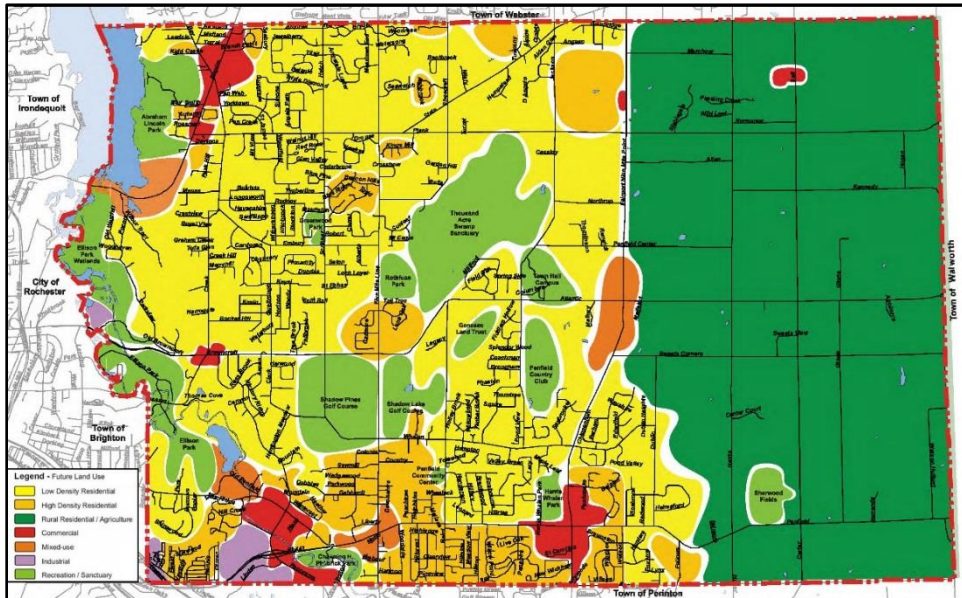


Figure 3.1. Future Land Use Map, Town of Penfield 2010 Comprehensive Plan Update.

## 3.1 Background

The Town of Penfield has a long history of planning for its future. This is evident in the Town motto, “The Town of Planned Progress,” and from the many studies and plans that have been completed over the years. The first Penfield Master Plan was prepared in May 1966, and recognized Penfield’s potential for growth over the next several decades.

The tradition of planning has continued over the decades as the Town of Penfield has updated its Comprehensive Plans approximately every 10 years, with the most recent plan completed in 2010. The Town has also conducted many traffic and transportation corridor studies, area plans, bike and recreation master plans, and economic development plans. The community intends to continue using its previous plans and studies as resources and direction for its future boards and community vision.



## 3.2 Past Plans and Studies

This section provides a brief overview of plans and studies that have led to the Town of Penfield's implementation of mixed-use zoning districts. The most influential study was the implementation of the 2010 Comprehensive Plan Update. All documents are available on the Town's website ([www.penfield.org](http://www.penfield.org)) under the "Online Documents" link. Contact the Penfield Planning Department for additional assistance.

### 3.2.1 2010 Comprehensive Plan Update

The purpose of the 2010 Comprehensive Plan was to provide a framework for future public and private investment. In addition to articulating a community vision, the plan established policies and strategies to achieve that vision.

The 2010 Comprehensive Plan identified three important considerations: (1) aging population, (2) fiscal responsibility, and a (3) mixed-use zoning district.

The Penfield 2010 Comprehensive Plan recommended the addition of "Mixed-Use" as a land use category in four (4) specific locations of the Town. These areas are symbolized by the orange figures on the Future Land Use Map that can be found in the 2010 Comprehensive Plan, as seen in Figure 3.1.

One of these locations included a 360 +/- acre portion of land on Fairport Nine Mile Point Road (NYS Route 250) from the Eastside YMCA northward to the intersection at Penfield Center Road.

See website: [http://www.penfield.org/media/dpt\\_planning\\_CompPlan\\_Final\\_Version.pdf](http://www.penfield.org/media/dpt_planning_CompPlan_Final_Version.pdf)

### 3.2.2 Comprehensive Plan Findings

The 2010 Comprehensive Plan steering committee, made up of stakeholders, determined observations and key objectives for mixed-use development in the Town of Penfield.

The Town of Penfield completed its most recent Comprehensive Plan in 2010. During the public participation component of the planning process, participants emphasized the need for residential options beyond the single-family residences now widely available throughout the Town. Mixed-Use was identified as an option offering a variety of residential development styles blended with community uses and neighborhood commercial. A safe transportation system offering options to accommodate a spectrum of people was also identified as desirable.

Throughout the public input process for the 2010 Comprehensive Plan, two issues were consistently raised: (1) the lack of residential diversity, and (2) the small amount of commercial development in the Town, resulting in an unbalanced tax base.

In response, one of the major initiatives within the 2010 Plan was to allow for additional mixed-use development within the Town. Excerpts from committee recommendations include:

## Observations

- (1) "There is an unmet need in the Town of Penfield and region for alternatives to traditional suburban tract housing that enhances "quality of life" with walkability, safety, proximity to jobs, services and entertainment, and higher standards for design and layout."
- (2) "Housing market demands have shifted and there is a desire for diverse neighborhood options."
- (3) "Rent levels for multifamily, office, and retail are relatively high in Penfield compared with the rest of the region; this is a sign of a fairly healthy demand for these types of products."
- (4) "Well-designed, higher-density development is more sustainable in terms of reducing the development footprint, and also more effectively uses costly infrastructure, reduces energy use and transportation costs, and increases access to amenities."
- (5) "Mixed-Use development patterns have been proven in other locations to be more sustainable than typical suburban conventional zoning."
- (6) "Mixed-Use development helps create a stronger economic base and produces more tax revenue for a community."
- (7) "A Mixed-Use Development District provides careful consideration of the public environment and its interaction with privately occupied spaces."

## Objectives

- (1) "A district that promotes a coordinated approach to access management, traffic circulation, multi-modal elements, interconnectivity, lighting, landscaping, and community character is desired."
- (2) "A transportation system that provides for interconnected streets, realistic travel options, and equal consideration to all modes of travel is desired."
- (3) "The general public, including multigenerational, singles and families, and people with diverse income levels, are to be accommodated."
- (4) "The built environment is designed and maintained with upstate New York's four seasons in mind. Particular attention is paid to design features that will encourage walkability even in inclement weather. Snow removal strategies will be addressed as part of all mixed-use developments."
- (5) "The commercial component of a Mixed-Use Development District is expected to be at a neighborhood scale and is not anticipated to serve regional markets."
- (6) "Mixed-Use zoning is sufficiently flexible to allow developers to respond to a variety of market conditions."

### 3.2.3 NYS Route 250 Traffic Analysis, 2022

In 2022, the Town of Penfield engaged the services of a consultant to evaluate the potential traffic impacts and required mitigation associated with the full potential build-out within the Mixed-Use Development District and nearby intersections just outside of the zoning district boundaries (e.g., NYS Route 250 and Whalen).

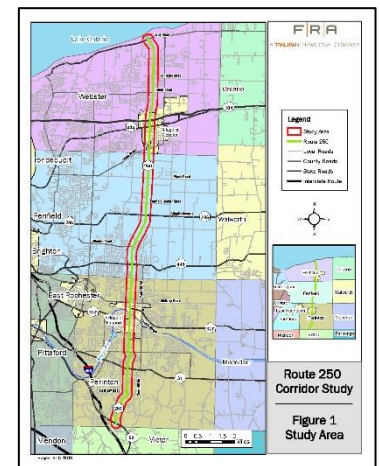
The traffic analysis identified a series of phased improvements that would be required to maintain safety and adequate levels of service as new developments are constructed and occupied.

### 3.2.4 NYS Route 250 Corridor Transportation Study, 2009

This study developed a long-range plan for the NYS Route 250 corridor to address the transportation needs of each linked community. The study specifically addressed current and future congestion problems, future growth, roadway improvement needs, access management strategies, and auto, truck, transit, bicycle, and pedestrian improvements.

Of the municipalities along the NYS Route 250 corridor, Penfield has the greatest amount of undeveloped land. Consequently, there is significant interest in proactive transportation planning to protect the character and safety of the NYS Route 250 corridor.

The study showed that although Penfield has significant zoning in place to limit development in much of the eastern part of the Town, the development of a coordinated land use and transportation plan would be highly desirable to help guide future land use decisions along the NYS Route 250 corridor. Further, NYS Route 250 at NYS Route 441 will continue to be a commercial hub and development pressures are likely to grow at this busy intersection.



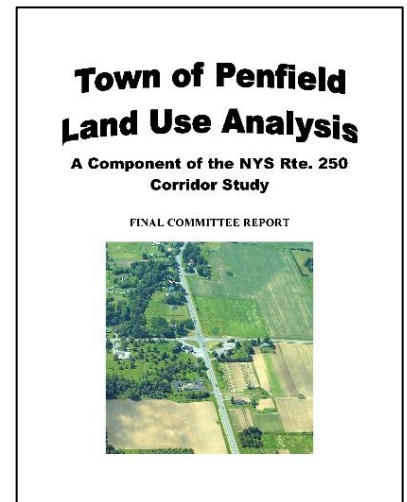
See website:

[http://www.penfield.org/media/dpt\\_engineering\\_Rt\\_250\\_Corridor\\_Study\\_Final\\_Report.pdf](http://www.penfield.org/media/dpt_engineering_Rt_250_Corridor_Study_Final_Report.pdf)

### 3.2.5 NYS Route 250 Land Use Analysis, 2009

This study analyzed the current and potential land use patterns of the NYS Route 250 corridor as a companion to the NYS Route 250 Corridor Transportation Study received by the Penfield Town Board in 2009. Key recommendations of the land use analysis include creation of an Access Management Overlay District to reduce the number of conflict points along the corridor and the development of a mixed-use “hamlet” area near the intersection of NYS Route 250 and Sweets Corners Road. The hamlet concept emphasized, “Creating a node of development largely focused on agribusiness with some potential for niche retail and/or hamlet-style housing mixed in.” The land use analysis was adopted in 2009.

See website: [http://www.penfield.org/media/dpt\\_planning\\_NYS\\_Route250CorridorStudyLandUseAnalysis.pdf](http://www.penfield.org/media/dpt_planning_NYS_Route250CorridorStudyLandUseAnalysis.pdf)



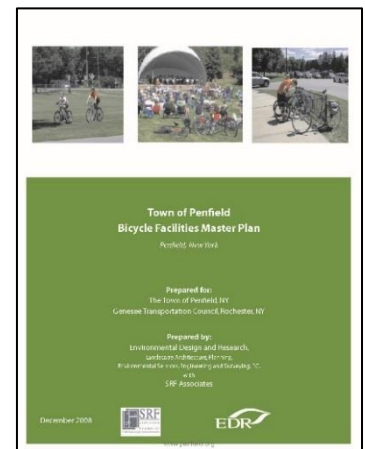
### 3.2.6 Town of Penfield Bicycle Facilities Master Plan, 2008

Through funding by the Genesee Transportation Council (GTC), the Town of Penfield contracted with Environmental Design and Research and SRF & Associates to design the Town’s Bicycle Facilities Master Plan in 2008. Recommendations for improvements were made in four categories: On-Road Improvements, Off-Road Improvements, Bike Facilities at Destinations, and Policies & Programs.

On-road and off-road improvements comply with The American Association of State Highway & Transportation Officials’ (AASHTO) 1999 Guide for the Development of Bicycle Facilities. A phasing plan and cost estimates are included to facilitate implementation of the recommendations. An education plan provides tools and strategies to increase public awareness, enhance safety, and encourage bicycling among a diversity of user groups. The Education Plan recognizes that transportation networks are shared resources utilized by motor vehicles, bicycles, and pedestrians alike. Maintenance is addressed as being of critical importance to bicycle comfort and safety.

The Bicycle Facilities Master Plan is a positive step towards making the Town of Penfield a bicycle-friendly community.

See website: [http://www.penfield.org/media/Bike\\_2008\\_Penfield\\_Bicycle\\_Facility\\_Plan.pdf](http://www.penfield.org/media/Bike_2008_Penfield_Bicycle_Facility_Plan.pdf)



### 3.2.7 Town of Penfield Recreation Master Plan, 2012

Recommendations of the Recreation Master Plan are designed to meet the mission and vision of the Department, as follows:

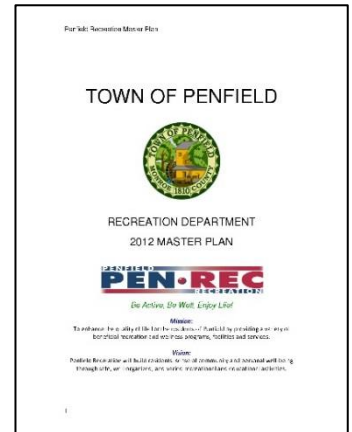
**Mission:** To enhance the quality of life for the residents of Penfield by providing a variety of beneficial recreation and wellness programs, facilities, and services.

**Vision:** Penfield Recreation will build residents' sense of community and personal well-being through safe, well-organized, and varied recreational and educational activities.

One of the five recommendations from the plan identified the need for the enhancement and/or acquisition of parks, recreation, and open space sites.

Characteristics of mixed-use support the goals of this plan through the principles of connectivity and promoting alternative modes of transportation by means of biking and walking throughout all developments.

See website: [http://www.penfield.org/media/2012 Recreation Master Plan.pdf](http://www.penfield.org/media/2012_Recreation_Master_Plan.pdf)



### 3.3 Application Process Overview

The Town requires a two-phase site plan review process for all new developments and any significant development in the future. This approach is intended to encourage an informed development process and ensure future developments meet the Town's standards and requirements. It also strives to streamline the application process by avoiding multiple iterations of full site plan submission sets.

Prior to submitting any application, applicants/developers shall meet with the Planning Department staff to evaluate the proposed development for any immediate feedback or recommendations. Staff will provide applicants with the application instructions and forms.

The Town's revised regulations contain general information about the review and approval process in the district. The Penfield Planning Department maintains the application materials associated with new development in the Mixed-Use Development district.

# 4 Planning and Design Best Practices

## How to Design a Successful Mixed-Use Development Project

This section provides direction for preparing designs for mixed-use development projects in the Town of Penfield in order to create continuity between projects in the Mixed-Use Development District. The standards listed below support the ten principles of mixed-use development described earlier in the Manual.

Mixed-Use developments seek to combine multiple uses within a compact site. The following logistical planning and design components should be addressed early in the process:

- (1) Ensure uses are complementary to create synergy and promote social interaction and community pride.
- (2) Connect uses by short and direct sidewalks or paved paths to make travel easier for pedestrians to move throughout the development.
- (3) Implement compact development in order to provide a walkable environment.
- (4) Plan for a maximum walking distance of roughly 1/8 of a mile (660 feet) to connect people to neighborhood amenities and services.
- (5) Design small, shared parking lots that support multiple uses throughout the day. Supplement parking areas with on-street parking to reduce the need for large expansive parking fields.
- (6) Ensure building architecture within a mixed-use development is complementary to create visual interest and continuity. Provide designs that are consistent in terms of building type, scale, density, and overall character.
- (7) Include transit stations to reduce dependency on the automobile.
- (8) Buffer existing neighborhoods to respect the natural boundaries of the area.
- (9) Implement designs that support economic development. A mixed-use development should include the flexibility to adjust sizes and layout of spaces in order to be able to adapt to a changing market.
- (10) Design a built environment that can support daytime programs and evening events. This includes uses that create a dynamic, vibrant setting and allow parking and other features to be shared.



**Figure 4.1.** The design standards for Lake Oswego in Portland, Oregon, help create a development that is consistent in terms of colors, textures, forms, and materials.

## Master Plan Requirement

Each development within the Mixed-Use Development District should include a conceptual master plan that provides sufficient detail to understand how the development would work and how it would relate to adjacent land uses. If a development is intended to be implemented in phases, each phase should be clearly defined. Each phase must meet the requirements of the Mixed-Use Development District.

### 4.1 Site Design and Planning Elements

Certain design elements are universal in all mixed-use developments. The combination of these elements and their implementation will be determined as part of a master planning process for each development in the Mixed-Use Development District.

In this section the following design and planning elements will be described with respect to the concepts, standards, characteristics, planning, and implementation in the development process:

- (1) Impact on proposed land uses
- (2) Mix of uses
- (3) Permitted uses
- (4) Specially permitted uses
- (5) Parking
- (6) Landscaping
- (7) Site furnishings

- (8) Public spaces
- (9) Signage/wayfinding
- (10) Lighting
- (11) Connectivity – pedestrian/vehicular/complete streets/multi-modal transportation
- (12) Social interaction
- (13) Public transit
- (14) Loading, storage, and trash disposal

The following sections in this chapter of the Manual provide details of how these elements can be incorporated into mixed-use development.

It is important to note that proposed developments conform to the Town Design and Construction Specifications, last revised in 2019, which provides the requirements for all roadway, stormwater, utility, and other infrastructure designs for mixed-use developments. In instances where the Town Design and Construction Specifications differ from the design requirements set forth in Town Code Section 250-5.12, the requirements in Section 250-5.12 shall prevail.

#### 4.1.1 Impact on Proposed Land Uses

The type of land use in a development will impact community resources such as roads, utility infrastructure, schools, parks, and recreation. All development will cause some level of impact, with the largest, most dense developments making the biggest impact.

Mixed-Use developments in the Town of Penfield will require an environmental assessment as prescribed by 6 NYCRR Part 617 State Environmental Quality Review (SEQR). [Statutory authority: Environmental Conservation Law Sections 3-0301(1)(b), 3-0301(2)(m) and 8-0113]. SEQR requires the sponsoring or approving governmental body to identify and mitigate the significant environmental impacts of the activity it is proposing or permitting. The SEQR Environmental Assessment Forms (EAF) are available on the DEC's website: <http://www.dec.ny.gov/permits/6191.html>.

#### 4.1.2 Mix of Uses

Successful mixed-use developments provide a balanced and vibrant mix of compatible uses. Developments within the Mixed-Use Development District are required to plan more than one use.

The mix of uses will vary among projects, and they will be evaluated within the context of the individual development and its relationship to the overall district. The number of required uses will be determined for each specific mixed-use development based on location (e.g., zone), size, characteristics, surrounding land use, and previous or planned development within the District. Further details can be found in the Mixed-Use Development District regulations in Town Code Section 250-5.12.





**Figure 4.2.** *Ivy Walk, located in Smyrna, Georgia, is a development that integrates vertical and horizontal mixed-uses.*

### 4.1.3 Permitted Uses

Chapter 250-5.12 of the Code of the Town of Penfield lists the permitted uses in the Mixed-Use Development District (also available in Appendix D).

### 4.1.4 Conditionally Permitted Uses

Conditionally permitted uses are identified as other uses not specifically listed as permitted uses but deemed by the board having jurisdiction to be similar in nature and compatible with the purposes of the district, that may also be considered. A list of conditionally permitted uses can be found in Chapter 250-5.12 of the Code of the Town of Penfield for the Mixed-Use Development District.

<b>Land Use Impacts on Travel (Litman 2006)</b>		
<b>Factor</b>	<b>Definition</b>	<b>Travel Impact</b>
Density	People or jobs per unit of land area (acre or hectare).	Increased density to reduce per capita vehicle travel. Each 10% increase in urban densities typically reduces per capita VMT by 2-3%.
Mix	Degree that related land uses (housing, commercial, institutional) are mixed.	Increased land use mix tends to reduce per capita vehicle travel, and increases uses of alternative modes, particularly walking for errands. Neighborhoods with good land use mix typically have 5-15% lower vehicle-miles.
Regional Accessibility	Location of development relative to regional urban center.	Improved accessibility reduces per capita vehicle mileage. Residents of more central neighborhoods typically drive 10-30% fewer vehicle-miles than residents of more dispersed, urban fringe locations.
Centeredness	Portion of commercial, employment, and other activities center.	Increased centeredness increases use of alternative commute modes. Typically, 20-50% of commuters to major commercial centers drive alone, compared with 80-90% of commuters to dispersed locations.
Connectivity	Degree that walkways and roads are connected and allow direct travel between destinations.	Improved roadway connectivity can reduce vehicle mileage, and improved walkway connectivity tends to increase walking and cycling.
Roadway design and management	Scale, design, and management of streets.	More multi-modal street design & management increases use of alternative modes. Traffic calming tends to reduce vehicle travel and increase walking and cycling.
Walking and cycling conditions	Quantity and quality of sidewalks, crosswalks, paths and bike lanes, and the level of pedestrian security.	Improved walking and cycling conditions increase non-motorized travel and can reduce automobile travel, particularly if implemented with land use mix, transit improvements, and incentives to reduce driving.
Transit quality and accessibility	Quality of transit service and degree to which destinations are transit accessible.	Improved transit service quality increases transit ridership and can reduce automobile trips, particularly for urban commuting.
Parking supply and management	Number of parking spaces per building unit or acre, and how parking is managed.	Reduced parking supply, increased parking pricing and increased application of other parking management strategies can significantly reduce per capita vehicle travel. Cost-recovery parking pricing typically reduces automobile trips by 10-30%
Site design	The layout and design of buildings and parking facilities.	More multi-modal site design can reduce automobile trips, particularly if implemented with improved transit services.
Mobility management	Various programs and strategies that encourage more efficient travel patterns.	Mobility management policies and programs can significantly reduce vehicle travel by affected trips. Vehicle travel reductions of 10-30% are common.

**Table 4.1.**  
*This table describes various land use factors that can affect travel behavior and population health.*  
<http://www.vtpi.org/t dm/tdm20.htm>

## 4.1.5 Parking

### A. Parking Concept

Shared parking is required within all mixed-use developments. It is most effective when land uses have different parking demand patterns that vary by time of day, week, or season. Shared parking reduces the amount of land needed for parking, creating opportunities for compact development, more space for pedestrian circulation, and increased open space and landscaping. Further, shared parking can reduce the impact of the automobile in land planning, which helps to protect cultural and natural resources.

### B. Parking Standards

The Town of Penfield uses the Urban Land Institute's (ULI) shared parking model to calculate parking demand for mixed-use and other developments. The ULI model is designed to project parking demand rates of a mixed-use development throughout a 6:00 a.m. to midnight cycle on typical weekdays/weekends each month of a year. Further, residential parking spaces are considered private and are not available for public use.

In 2007, the Monroe County Planning Department conducted a parking study and produced statistical analyses of parking needs by land use. The purpose of this project was to obtain accurate parking data for analysis to determine parking ratios for various land uses. The parking data and analysis was aimed at aiding interested municipalities in the evaluation of current parking standards for future development and provide current parking data for developers and consultants. The Town of Penfield has utilized the data from this document in the past in order to provide guidance to its boards and developers to assist in properly sizing parking lots, based on the use of the parcel.

Parking criteria for each mixed-use development will be evaluated independently based on site layout and nature of uses within the development.

### C. Parking Design Characteristics

A key design characteristic of a mixed-use development is shared parking. Designing the location, orientation, and appearance of parking lots, garages, and on- and off-street parking spaces within a development is critical for creating walkable neighborhoods and a sense of community.

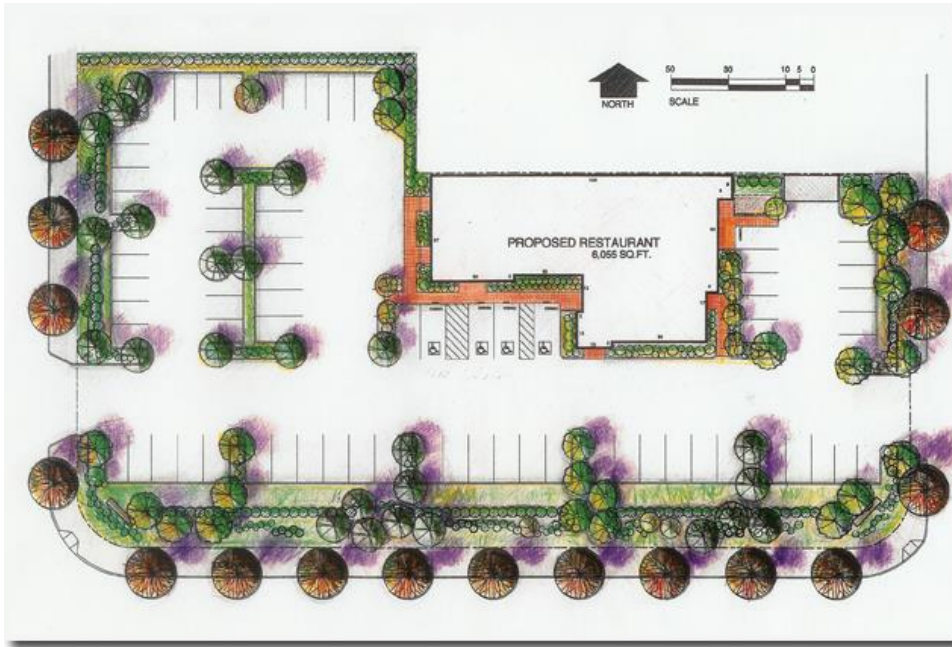
The following characteristics contribute to parking layouts that are shared, compact, and discreet:

Types:

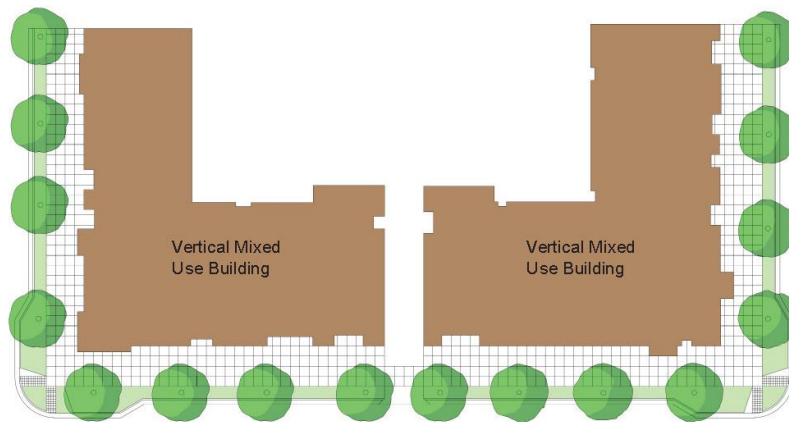
- (1) Shared use – Shared parking areas linked to different uses by way of pedestrian walks and trails. Day/night use of the same parking areas by complementary users.
- (2) Park and walk – A single automobile trip to visit multiple land uses.
- (3) Compact parking area – Multiple small lots instead of one vast parking lot.

(4) Reduced parking stall sizes – Small parking spaces (9'x18') use parking areas more efficiently. This size may be reduced in cases where angled parking is being proposed or in areas where small vehicle designations are made.

(5) Land-banked parking – Reserved green space for overflow or “land-banked” parking supports future growth and allows pavement to only be installed as needed.



**Figure 4.3.** This plan view shows planting concepts as well as parking layout.



**Figure 4.4 - 4.5.** These vertical mixed-use examples would be appropriate for most of Penfield's mixed-use developments. The visual impact of the three-storied structures is reduced by the use of strong horizontal lines, a change of colors and materials, and the introduction of pedestrian scale elements such as street trees.



## Locations

- (1) Rear parking – Parking behind structures reduces the visual impact of parking lots and provides visual connectivity to adjacent land uses.
- (2) On-street parking – Meets parking needs and encourages traffic calming.
- (3) Alleys – Alleys provide access to rear-entry garages and parking lots along structures.
- (4) Parking structures – Vertical parking structures maximize land use in dense areas.

## Features

- (1) Landscaping – Trees, shrubs, and groundcover help reduce the visual impact of parking lots.
- (2) Bicycle racks – Bike storage supports use of bicycles for transportation.
- (3) Valet parking – Parking by attendants keeps parking organized and convenient for customers.

### D. Implementation of Shared Parking

A shared parking plan can be a stand-alone plan or submitted as part of a site plan, landscape plan, or other plan that communicates the layout of the mixed-use development.

A shared parking plan could include one or more of the following:

- (1) Site plan of parking spaces intended for shared parking and their proximity to the land uses they will serve.
- (2) Signage plan that directs drivers to the most convenient parking areas for each particular use or group of uses (if such distinctions can be made).
- (3) Pedestrian or vehicular circulation plan that shows connections and walkways between parking areas and land uses. Walkways should be direct and easy to use.
- (4) Safety and security plan that addresses lighting and maintenance of the parking areas.

Use	Weekday			Weekend	
	NIGHT - Midnight to 7 a.m.	DAY - 7 a.m. to 5 p.m.	EVENING - 5 p.m. to Midnight	DAY - 6 a.m. to 6 p.m.	EVENING - 6 p.m. to Midnight
Office & Industrial	5%	100%	10%	10%	5%
Retail	5%	90%	50%	100%	70%
Hotel/Motel	70%	70%	100%	70%	100%
Restaurant	10%	50%	100%	50%	100%
Restaurant Associated w/ Lodging	10%	50%	60%	50%	100%
Entertainment & Recreation	10%	40%	60%	50%	60%
Day Care	5%	100%	20%	20%	5%
Housing	100%	60%	100%	100%	100%
All Others	100%	100%	100%	100%	100%

**Table 4.2.** This table from ULI shows general parking demand for different uses. For example, a parking area for office/industrial used 100% during weekday business hours, but the same parking area has low use during evenings/nights and weekends.

Implementation of shared parking occurs through legal agreements between adjacent uses. Shared parking and cross access agreements allow for all uses to have access to parking spaces at any given time.

The future maintenance of parking facilities is privately managed by one or more property managers and/or owners.

## 4.1.6 Landscaping

### A. Landscaping Concept

Landscape design can enhance the overall aesthetic value and character of a mixed-use development. Landscaping can be used to define spaces, direct pedestrian traffic, screen unwanted views, provide shade, and enhance walkability.

### B. Landscaping Standards

The Town of Penfield landscape requirements are subject to review and approval of the Planning Board and apply to all developments. An additional level of landscape and hardscape detail should reflect the unique character of each mixed-use development. Plantings should provide visual interest throughout all four seasons to sustain a vibrant, appealing landscape.

### C. Landscaping Characteristics

Landscape design should integrate aesthetic and functional requirements of the mixed-use development with the native plantings found in upstate New York. Plant species, sizes, colors, layouts, and hardscape materials reinforce the overall design theme, scale, and

visual connection of the district. Mature trees shall be preserved and integrated into the project design unless proven to be unfeasible. Plantings should be low maintenance and suitable for the four-season climate.

#### D. Landscape Planning

A landscape plan, designed by a certified landscape architect, shall be included within the site plan designs for all mixed-use development projects. The board having jurisdictions may defer the review of landscape plan to the Town's landscape consultant.

#### E. Landscaping Implementation

Landscape design should consider the scale and mass of a building and its relationship to the street and neighboring properties.

### 4.1.7 Site Furnishings

#### A. Site Furnishing Concept

Site furnishings, such as benches, bike racks, lighting, and decorative bollards are an essential part of mixed-use developments because they reinforce social interaction, walkability, and pedestrian scale activities. Furnishings provide visual continuity and help establish the overall character of a development.

#### B. Site Furnishing Characteristics

All street furniture including benches, lighting, bollards, and waste receptacles should be consistent and complementary with the architectural style and quality of the buildings that are part of a mixed-use development. Elements such as masonry walls, boulders, sculptural elements, amphitheaters, and architectural features can serve as formal or informal places for seating and gathering.

Art in public areas adds visual interest and helps create a sense of identity. The use of creative art in public areas may include sculptures, mosaics, murals, decorative carving, ornamental benches, decorative paving, interactive mobiles, and other elements of a unique and attractive nature that embodies artistic qualities.

Additionally, pet-friendly drinking fountains and pet waste bags stations will encourage residents to walk.

#### C. Site Furnishing Planning

Benches and informal seating areas should be designed to encourage people to sit and enjoy public spaces. Benches should be placed and oriented to provide refuge and comfort, but not impede access or circulation. They should also be durable and comfortable. Some benches should be provided with arms to assist older people.



**Figure 4.6.** Each mixed-use development has a maximum height restriction. There are techniques for reducing the perceived height of an architectural structure. Using variations in colors, textures, and materials, will help visually break up the perceived scale of a building. Introducing street trees, awnings, benches, seating areas, and other street furnishings also helps create a pedestrian scale and reduces the visual impact of structures.

Bike racks should be placed in convenient locations to encourage the use of bicycles. Trash receptacles, loading areas, and mechanical equipment should be located away from public spaces and screened by architectural building features, fencing, or landscaping to minimize negative impacts.

Refuse enclosures should be self-closing with self-latching doors. Any enclosure should accommodate all refuse dumpsters, totes, and recycling bins. Trash and recycling bins shall be shared among users to minimize the number of enclosures throughout the development.

#### D. Site Furnishing Implementation

- (1) Site furnishings shall be identified on the site plans for each development.
- (2) All outdoor seating areas shall leave at least five (5) feet of unobstructed pedestrian space.
- (3) Graffiti resistant material and/or coating and skateboard deterrents shall be used to reduce the possibility of vandalism.

### 4.1.8 Public Spaces

Centrally located public open space that encourages social interaction is critical to mixed-use developments. Traditional Town squares, urban plazas, parks with walkways, shade trees, benches, and other similar uses all help to spatially organize mixed-use developments. Many Mixed-Use Development District requirements call for a minimum of public open space to provide for active recreation, programmed uses, events and activities that create a community focal point and preserve environmental features.

Mixed-Use development plans must integrate existing and planned usable public space in ways that benefit the overall mixed-use development as well as the public at large. Ideally, public open space should be made available to all dwelling units within mixed-used developments.



All public open space (active and passive recreation areas, public courtyards, Town square, and other areas that could be used for public gatherings) should be protected from future redevelopment, unless it's identified clearly as land banked open space for an identified future development need (e.g., land-banked parking).

#### A. Public Space Characteristics

Design elements that exemplify public spaces:

- (1) Centralized open areas
- (2) Town square / village green
- (3) Connections for pedestrian accessibility
- (4) Sheltered areas
- (5) Benches and seating
- (6) Recreation space
- (7) Community gardens
- (8) Shade trees and plant material
- (9) Wildlife habitats

#### B. Public Space Planning

For planning purposes, new public spaces in mixed-use developments should integrate with existing, adjacent public spaces. Public spaces will also mix with and complement new shared use spaces such as parks, Town squares, civic buildings, green spaces, transit stops, sidewalks, plazas, and similar uses.

#### C. Public Space Implementation

In many mixed-use developments, public spaces are managed by an association that oversees decisions on day-to-day operations. This association could include business owners and residents.



**Figure 4.7 - 4.9.**  
*Vintage Walk (top, above) in Covina, California, integrates residential development with office space and public open space. Community gardens are available in this development in Seattle, Washington (bottom, above). Multi-use trails (left) can link mixed-use developments to surrounding parks, recreational facilities, and other public facilities.*



## 4.1.9 Signage/Wayfinding

### A. Signage and Wayfinding Concept

A cohesive signage and wayfinding program creates a sense of identity for each mixed-use development. All signage should be designed to complement the architectural style and setting of adjacent structures.

### B. Signage and Wayfinding Characteristics

Permanent signs should be constructed of high-quality materials such as metal, stone, or wood, and sign letters and materials should be professionally designed and fabricated. The signage program should provide a framework for clear, effective, visible, safe, and aesthetically pleasing identification and directional communication.

### C. Signage and Wayfinding Recommendations

Signage within a mixed-use development should utilize the same design standards in order to ensure a visually cohesive environment. A sign program should identify the placement, size, materials, colors, method of lighting, and other related requirements for all signs.



**Figure 4.10.** An information kiosk use for wayfinding and the sharing of public information.

Directional signage should be located at major vehicular and pedestrian access points.

Directional signage for vehicles and pedestrians, entry signage, and building identification should be horizontal in format and installed lower to the ground in order to improve readability and minimize visual impact to the surroundings. Building-mounted signs may be wall-mounted, projecting, or combined with awnings.

Signs may be illuminated as long as they do not contribute to light pollution.

Animated, moving, flashing, blinking, reflecting, and revolving signs are not appropriate in mixed-use developments. Restricted and prohibited signage are outlined in the Town Code.

### D. Signage and Wayfinding Planning

The Planning Board is responsible for approving the initial sign package for a development. Any subsequent modification or alteration to the originally approved sign package will require review from the Penfield Zoning Board of Appeals.

## 4.1.10 Lighting

### A. Lighting Concept

Lighting contributes to the overall character and safety of a mixed-use development. A mixed-use development with a comprehensive lighting plan is generally perceived as safe, accessible, and inviting to residents and visitors.

### B. Lighting Recommendations

Street lighting in public rights-of-way should be provided in accordance with existing Town of Penfield standards. Current standards for lighting emphasize aesthetics, architectural qualities, and energy efficient fixtures that minimize light pollution.

### C. Lighting Characteristics

Light fixtures should be harmonious across a mixed-use development to create an overall safe, inviting, and attractive setting.

Interior public space lighting should complement the surrounding streetscape and architecture and be incorporated into surrounding design elements. Sidewalks, walkways, and pedestrian gathering spaces within mixed-use developments should be lit for safety and aesthetics.

### D. Lighting Planning

The following attributes should be considered in the planning phase:

(1) Observe dark sky principles to the greatest extent feasible. In more rural areas lighting impacts should be reduced by dimming at night and choosing a low lighting classification.

(2) Establish operational hours for light fixtures that provide safe lighting levels during hours of operation for primary and adjacent facilities.



**Figure 4.11.**  
*Lighting along this street in Schenectady, New York, helps create a safe, harmonious walking environment.*

- (3) Emphasize the use of low-power luminaires for exterior lighting to illuminate and provide reflected luminance of objects.
- (4) Manage light pollution by installing cut-off light fixtures that direct light down.
- (5) Include lighting along roadways and within mixed-use developments for safety and security.
- (6) Vary the heights of pole lighting within developments based on the intent of use.
- (7) Use LED light sources where possible throughout developments.

## 4.1.11 Connectivity

Connectivity is the principle of connecting people, vehicles, and places to reduce traffic, create walkable street and trail networks, and promote sociability. It is a fundamental component of mixed-use development.

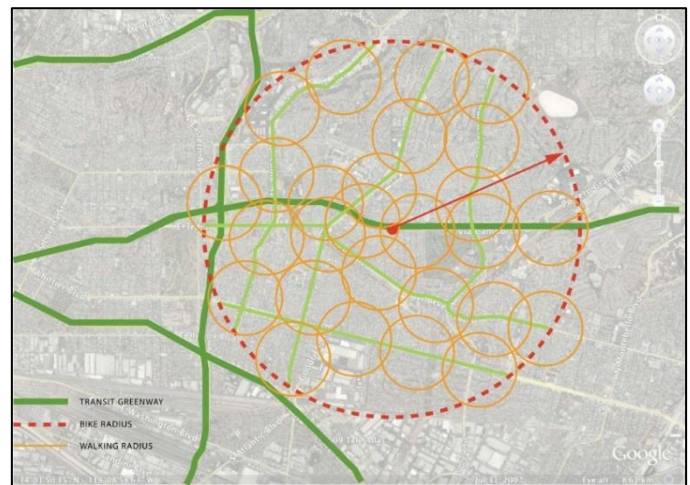
The following sections address pedestrian, multi-modal and vehicular connectivity. They also provide explanations of interconnected and complete streets, which are required for site plans proposed within the mixed-use development.

### A. Pedestrian Connectivity and Walkability Concepts

Walkability is accomplished when mixed-use developments connect people and places with direct and easy to use routes. Walking improves health for both young and old, promotes social interaction, reduces dependency on cars, and reduces energy demands. The more people walk, the less they use their cars, and the less impact there is on existing roadways. Placing different uses in close proximity makes it easy to walk from one location to another.

Sidewalks facilitate this by connecting common public areas, parking areas, storage areas, open space, and recreational facilities within the mixed-use development. The level of walkability in a mixed-use development depends on the proximity of structures.

Compact development clusters buildings into a compact area that requires less infrastructure than traditional zoning. This type of site planning creates a more walkable area for travelers.



**Figure 4.12.** The orange lines indicate a one-quarter mile walking distance that is recommended to help create a walkable mixed-use development.

### B. Pedestrian Connectivity and Walkability Recommendations

Design standards for pedestrian connectivity are as follows:

- (1) Sidewalks are needed along all road frontages to connect building entrances, parking areas, central open space, and other pedestrian destinations.
- (2) Clear and direct routes from on-site parking to a building entry and public sidewalk system.



**Figure 4.13.** Roads are an important part of mixed-use developments. This development along 15th Street in Atlanta, Georgia, has narrower roads to provide room for wider sidewalks that improve walkability.

(3) Commercial/retail development located along road frontages with sidewalk access.

(4) Pedestrian sidewalks adjacent to commercial buildings, offices, or storefronts shall be installed according to the Mixed-Use Development District Zoning Ordinance.

(5) Major multi-use walkways and trails intended to accommodate pedestrians, cyclists, and other users shall be installed according to the Mixed-Use Development District Zoning Ordinance.

(6) Easements shall be provided for the continuation of trails where they leave the mixed-use development.

(7) Proposed plans should incorporate decorative seating and benches along walkways.

For additional requirements, review the Mixed-Use Development District regulations contained in Section 250-5.12.

### C. Pedestrian Connectivity and Walkability Characteristics

Pedestrian paths should be accessible, attractive, inviting, and provide direct connections between places. Pedestrian elements in mixed-used developments include:

- (1) Sidewalks along all streets
- (2) Wide sidewalks to accommodate outdoor dining or events
- (3) Connection to adjacent uses
- (4) Multi-use trails
- (5) Lighting
- (6) Underpasses and overpasses

- (7) Street furniture
- (8) Textures, colors, and decorative crosswalks
- (9) Public art at pedestrian scale

#### D. Pedestrian Connectivity and Walkability Planning

Within a mixed-use development, a zero setback from the side property line(s) is encouraged to help create a more compact development and an uninterrupted pedestrian zone by avoiding excessive side yard setbacks between buildings. Minimum setbacks shall be required as specified to meet fire and building codes.

The number of curb cuts for vehicular entry should be minimized so that pedestrian and bicycle areas are safe, secure, and passable.

Any paving pattern, color, and material used to articulate pathways and pedestrian areas should continue when driveways intersect with these areas. Where pedestrian circulation paths cross vehicular circulation paths, a material change, contrasting color, or raised crossing should be used to delineate the continuing pedestrian path.

Sidewalks and/or plazas should be provided with weather protection (e.g., awnings/canopies) wherever possible.

Multi-use trails that link residential areas and public gathering spaces with parks and public open space are an essential element of larger mixed-use developments.



**Figure 4.14 - 4.15.** Examples of combinations of vertical mixed-use structures, single-family or multi-family residential development, and public open space enhance walkability (top). 15th Street in downtown Atlanta, Georgia (bottom), is a high-density development with commercial uses along the lower floors and a combination of office and residential uses above.



**Figure 4.16.** Various examples of pedestrian and bicycle friendly environments.



Successful walkways use overhead shelters to provide shade and protection from the weather. Shelters are often located at bus and transit stops, recreation areas, and other public gathering spaces.

## E. Vehicular Circulation

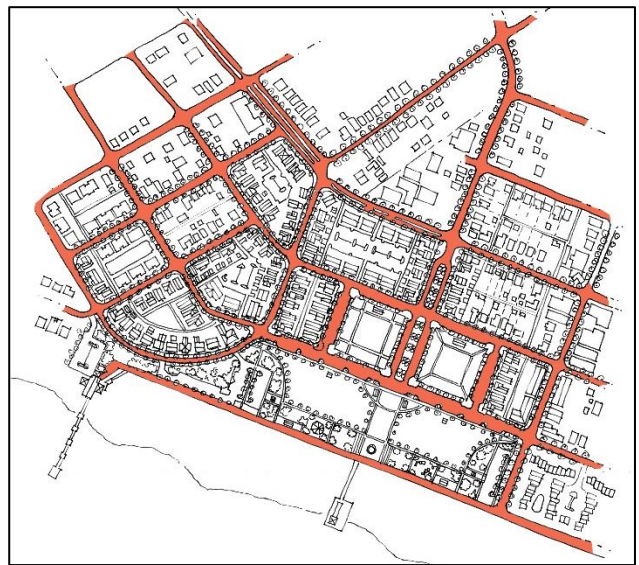
Mixed-Use developments are designed to reduce car use, traffic congestion, and parking. Mixed-Use developments can help to minimize demand for vehicle use by encouraging more internal trips and promoting walkability and alternative forms of transportation. The Town will support the use of car share programs as they develop in and around the community.

## F. Interconnected Streets

Successful mixed-use developments feature interconnected streets laid out as a grid or modified grid. This type of street system moves traffic equitably among all streets, gives drivers alternative routes to reach their destinations, and establishes a sense of community by allowing all streets to lead to community focal points.

Characteristics of interconnected streets:

- (1) Streets connected for multiple access routes
- (2) Grid pattern layout to create a greater number of entry and exit points
- (3) Minimal dead ends or cul-de-sacs to keep traffic fluid
- (4) Traffic calming devices to slow traffic
- (5) On-street parking to promote access
- (6) Minimal conflict with pedestrians to create a safer community



**Figure 4.17 - 4.18.** Street layout has to take into account snow removal, snow storage, and alternative routes in case one street or neighborhood is snowed in (below, top). This street layout plan (below, bottom) shows the type of interconnected streets, lot layout, and integrated open space that helps make mixed-use developments more walkable communities.

## G. Complete Streets and Streetscapes

The NYS DOT Complete Streets Act of August 15, 2011, defines a complete street as “a roadway planned and designed to consider the safe, convenient access and mobility of all roadway users of all ages and abilities: pedestrians, bicyclists, public transportation riders, and motorists; it includes children, the elderly, and persons with disabilities.”

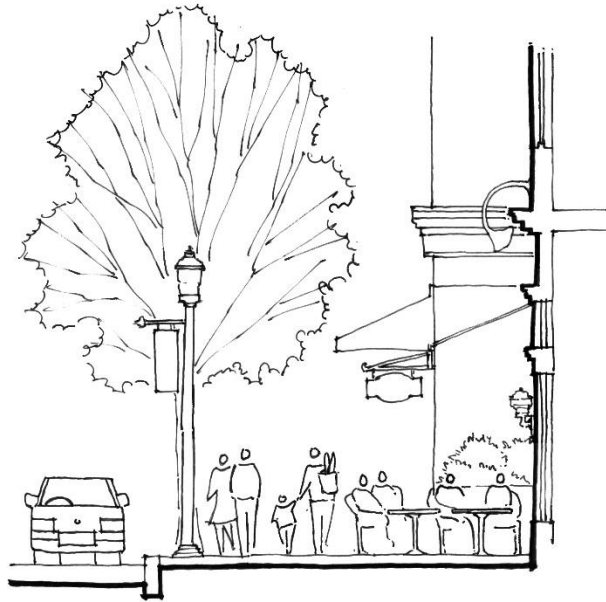
Characteristics of a complete street include:

Pedestrians:

- (1) Limited front setbacks
- (2) Wide sidewalks
- (3) Pedestrian control signals
- (4) Crosswalks
- (5) Wayfinding/signage
- (6) Curb cuts to accommodate all mobility needs
- (7) Protected bicycle lanes

Transportation:

- (1) On-street parking
- (2) Paved shoulders suitable for use by bicyclists
- (3) Lane striping for bike lanes, transit stops
- (4) Bus lanes and pull-outs
- (5) Median islands, curb extensions, narrow travel lanes, and roundabouts
- (6) Bicycle racks



**Figure 4.19.** Adding street trees, awnings, textured pavement, and other site furnishings can help reduce the scale of a roadway and slow traffic.



**Figure 4.20.** These streets in Overlook Village, Redmond, Washington, are based upon the city's Complete Streets Plan and Guidelines.

Décor

- (1) Street trees
- (2) Street furniture
- (3) Decorative lighting
- (4) Planting beds

H. Transportation Alternatives

Mixed-Use developments should offer a range of transportation options that de-emphasize dependence on personal automobiles. Transportation alternatives are explained below.

- (1) Sidewalks

The design and placement of sidewalks are a critical feature in mixed-use development. When properly implemented, they can open up new paths of travel for all types of non-automobile commuters.

See section 4.1.11 *Pedestrian Connectivity and Walkability* for more sidewalk details.



**Figure 4.21.** The City of Chattanooga, Tennessee, is one of many communities that provide bike rentals to encourage alternative modes of transportation. Penfield may not be ready to implement this type of program at this time, but there may be opportunities in the future if more multiuse trails are implemented.

## (2) Bicycle lanes

Mixed-Use developments rely on the use of complete street designs to move commuters safely and efficiently through the area. A key element is the use of bicycle lanes along roadways. Depending on the location, bicycle lanes and amenities will be required by the Town.

## (3) Public Transit

Transit stops should be included in the development whenever feasible. See section 4.1.13 *Transit Stops* for more details.

## (4) Electric Vehicle Charging Stations

Sales of electric vehicles have become more popular as commuters make an effort to reduce their carbon footprint. The Town of Penfield currently has three electric car charging stations in use. Beginning in 2012 the Town of Penfield began installing free public EV stations. The Town is committed to encouraging developers to consider installing electric car charging stations in parking areas for all future development proposals including, but not limited to, mixed-use development.

The implementation of alternative means of transit will vary for each mixed-use development application due to the location and scale of the proposal.

## 4.1.12 Social Interaction

Mixed-Use developments promote social interaction. Traditional Town squares, urban plazas, parks with walkways, shade trees, benches, and other similar features help to spatially organize mixed-use developments and promote social interaction.

The following design elements and uses encourage social interaction:

- (1) Public open space
- (2) Squares, plazas, and courtyards
- (3) Covered walkways
- (4) Street furniture and seating
- (5) Information kiosks
- (6) Public events, such as a farmers' markets
- (7) Programmed classes and activities



**Figure 4.22.** Vertical mixed-use structures with retail and commercial services (e.g., restaurants) on the bottom floor and residential on the top floors are appropriate for Penfield's MUD.



**Figure 4.23.** The Public Market in Rochester, New York, includes shaded booths that help define public spaces for visitors.

## 4.1.13 Public Transit

Transit stops should be included in the development whenever feasible. They can be located adjacent to pedestrian-oriented amenities such as courtyards, plazas, and parks. The applicant must contact the transit authority prior to making an application to determine what transit infrastructure would be appropriate for a given site.

#### 4.1.14 Loading, Storage, and Trash Disposal

All loading areas and loading docks should be located to the side and rear of buildings. Outdoor storage or display of materials should not be permitted overnight.

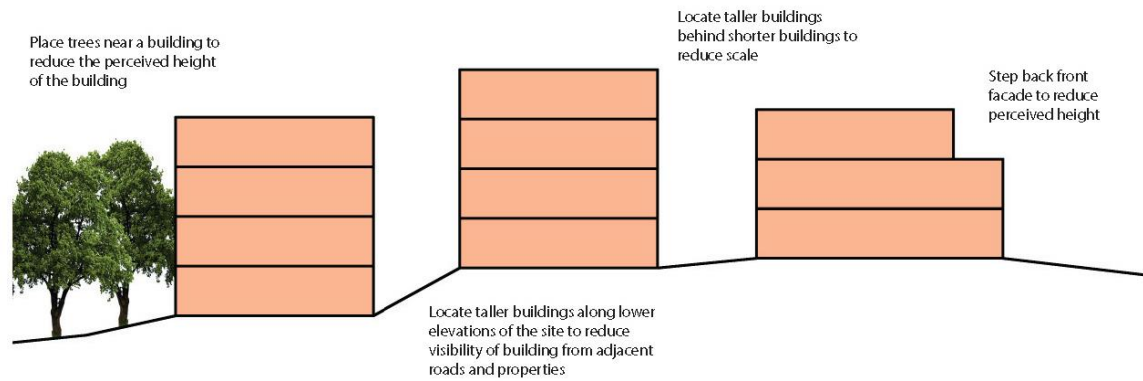
Trash disposal areas should be located within buildings or within an opaque screened area that hides the trash and is located to the side or rear of a building. Trash receptacles, loading areas, and mechanical equipment shall be located away from public spaces and screened by architectural building features, fencing, and landscaping to minimize negative impacts.

### 4.2 Architectural Design Practices

The architectural character within the Mixed-Use Development District should create a distinctive Town or village environment. Architectural styles for mixed-use developments in Penfield should reflect the community's historic character.

New buildings should be similar to existing regional structures in architectural style, materials, scale, and form. Visual interest is established with variations of color, texture, and architectural detail among adjacent structures. Architecture should include:

- (1) Existing architectural character of the neighborhood/district
- (2) Staggered building frontages
- (3) Street-level emphasis
- (4) Rhythm of windows and doors
- (5) Use of canopies to provide weather protection, human scale, accent materials, and color
- (6) Continuity of building size and scale
- (7) Variety of roof forms
- (8) Variations in building height and massing (rooftops can provide usable outdoor space for both residential and commercial developments)
- (9) Placement of architectural icons and preservation of landmarks to establish a strong spatial hierarchy that promotes a sense of place
- (10) A balanced relationship of buildings to public spaces
- (11) Indoor/outdoor spaces through structures, paving and glazing
- (12) Sustainable materials such as reclaimed wood and items made from recycled products
- (13) Durable materials such as concrete, metal, masonry, stone, glass, and tile



**Figure 4.26.** A diagram showing how multi-story buildings can be placed on a development site using the existing or proposed topography.

Photographs of successful Mixed-Use Development District can be viewed on the following pages.







Colorado Springs, CO

**Figures 4.30 - 4.31** Architectural styles may vary among mixed-use districts in Penfield, but the basic principles are consistent. Buildings should be consistent in scale, building materials, building height, and architectural details. The style may be modern or more traditional, but brick, stone, wood, and other traditional materials help ensure the buildings “fit” the aesthetic of the Town of Penfield.



British Columbia, Canada



**Figures 4.32 - 4.37.** These vertical mixed-use buildings (top left, top right, and middle left) have retail services on the bottom floor and residential on the top floor. A development in Jacksonville, Florida (top left) includes a cafe on the lower floor. A range of architectural styles and details, in general, are appropriate for mixed-use developments in the Penfield Mixed-Use District. These examples included stacked row houses (middle right and bottom row).



St. Johns, FL



Langley, WA



Niagara, NY



Gilroy, CA



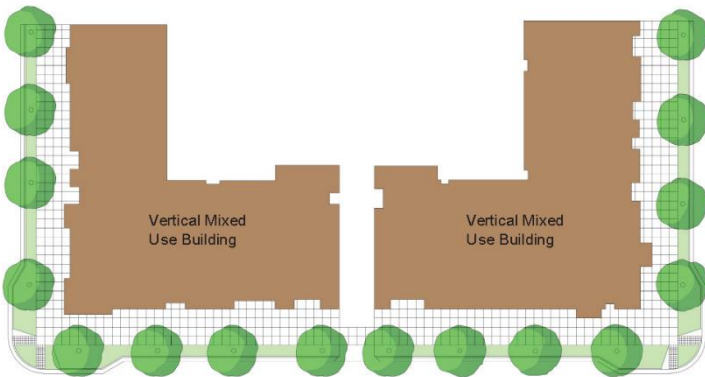
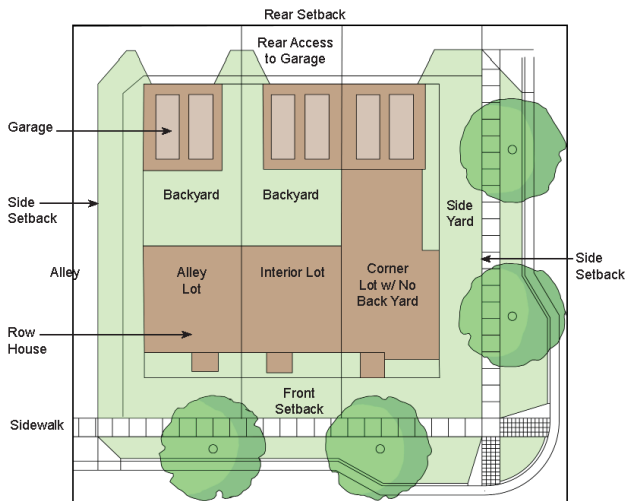
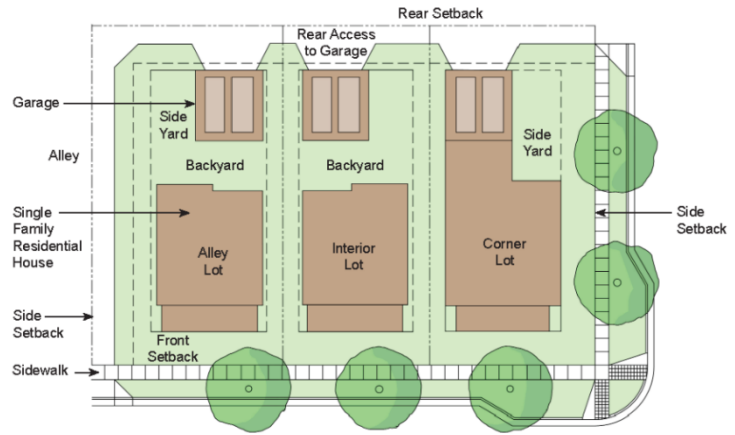
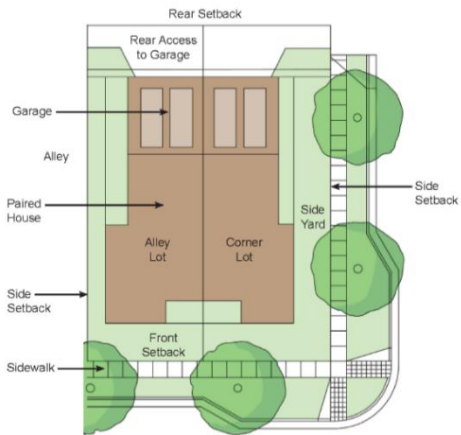
Philadelphia, PA



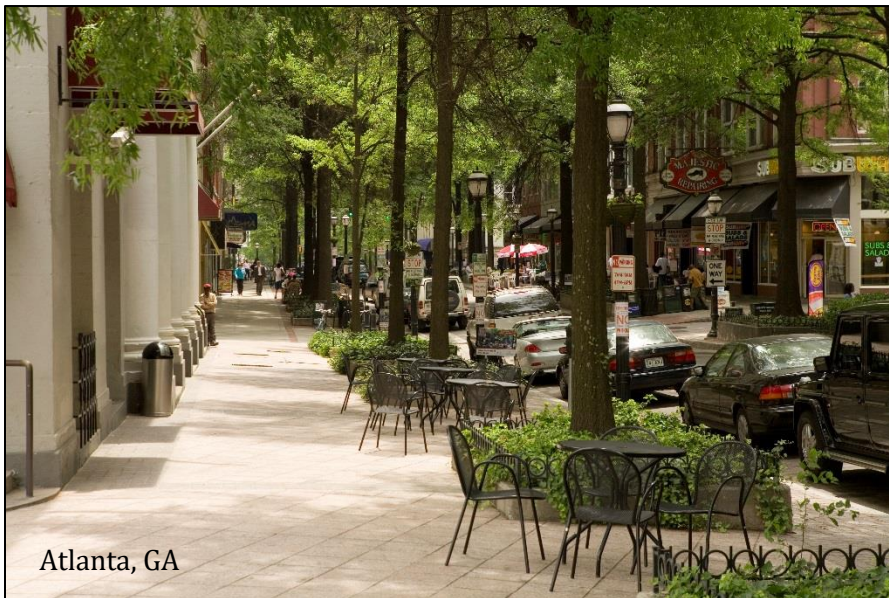
Denver, CO

**Figures 4.38 - 4.43.**

*Mixed-Uses that focus on commercial or retail (top left, middle rows, and bottom rows) provide a more diverse economic base. Smaller cottages (top right) on small lots and centered around public gathering spaces can be used to provide more affordable housing while increasing density. These provide examples of attached housing in various architectural styles that are historic in nature. The areas around buildings should be developed at a pedestrian scale in order to enhance walkability.*



**Figures 4.44 - 4.48.** Structures along property edges have traditional front, back, and side setbacks in order to minimize impacts to adjacent developed areas. Shared walls can also be used to reduce a building footprint. Vertical mixed-use buildings (bottom two rows) can use shared parking located behind the buildings.



**Figures 4.50 - 4.52.** Maximum height restrictions apply to every development. There are techniques for reducing the perceived height of a building including the use of variations in colors, textures, and materials to help visually alter the scale of a building. Buildings are often designed with a stepped back technique which helps to maintain a pedestrian scale while allowing for a higher building density. The use of street trees, awnings, benches, seating areas, and other street furnishings helps create a pedestrian scale and reduces the visual impact of structures.

## 4.3 Sustainable Practices

All mixed-use developments should incorporate sustainable design, planning, construction, and maintenance practices to the extent practicable. This applies to site features, engineering facilities, and architectural structures.

Sustainable design practices:

- (1) Preserve existing architectural and cultural structures within a district
- (2) Minimize impact on surrounding natural and cultural resources
- (3) Limit development to the smallest possible footprint
- (4) Adopt Low Impact Development (LID) methods such as bio-swales, rain gardens, constructed wetlands, and stream restorations
- (5) Observe International Green Building Code (IGBC)
- (6) Consider building designs that meet or exceed a Leadership in Energy and Environmental Design (LEED) Certification
- (7) Implement stormwater best management practices
- (8) Use green, sustainable materials (local, recycled, etc.) and practices



**Figure 4.53.** Low impact development solutions include open space that be used for stormwater management, as recreational spaces, and for storage of snow removal.

- (9) Incorporate green infrastructure (Required by NYSDEC)
- (10) Use renewable energy technologies to reduce carbon footprint
- (11) Preserve, protect, and enhance natural environmental systems
- (12) Maximize biodiversity with connected habitats and native plants
- (13) Preserve and protect healthy soils
- (14) Protect significant habitats
- (15) Remove invasive species



**Figure 4.54.** A combination of gravel to emulate the pattern of a natural creek bed and native materials help manage stormwater runoff while also improving aesthetic quality.



## 4.4 Environmental Concerns

All projects must comply with the U.S. Environmental Protection Agency (EPA) stormwater regulations, New York State Environmental Quality Review Act (SEQRR, NYSDEC regulations) and the Town of Penfield Environmental Protection Overlay Districts (EPODs).

Mixed-Use developments in Penfield are expected to emphasize environmental sensitivity and sustainability in all phases of design, planning, and implementation. To achieve this, all environmental regulations applicable in the State of New York and in the Town of Penfield will be applied to mixed-use developments and districts.

**Figures 4.55 - 4.56.** *The Town of Penfield has a rain garden located along Jackson Road that provides a good example of how to sustainably address stormwater runoff. These images show a new planting (top) and the same garden after several years (bottom).*



For mixed-use development, natural and cultural resources can be preserved for public use and enjoyment by cluster development and minimizing paving and infrastructure. Natural

resources include existing vegetation, water resources, wildlife, habitat areas, air quality, soils, and environmental systems and processes. Cultural resources include manmade elements such as historic features, built structures, trails, archaeological sites, and other similar features.

According to the EPA, one of the greatest areas of environmental concern for human health in developing and developed countries worldwide is water quality. Water is essential for life and plays a vital role in the proper functioning of the Earth's ecosystems. The pollution of water seriously impacts all living creatures and can negatively affect the use of water for drinking, household needs, recreation, fishing, transportation, and commerce.

Mixed-Use developments in the Town of Penfield will utilize best practices to protect water quality. To achieve this, developers are encouraged to implement Low Impact Development (LID) principles to minimize the potential negative impact of development on natural systems. LID helps communities protect water quality, habitat, and biological resources from the impacts of development and stormwater runoff. LID encourages the integration of treatment and management measures at the site level. In short, LID practices manage stormwater in a way that imitates the natural hydrology of a site.

The Natural Resources Defense Council recommends the following common LID practices:

- (1) Impervious surface reduction and disconnection
- (2) Permeable pavers
- (3) Pollution prevention and good housekeeping
- (4) Rain barrels and cisterns
- (5) Rain gardens and bio-retention
- (6) Roof leader disconnection
- (7) Rooftop gardens
- (8) Soil amendments
- (9) Tree preservation
- (10) Vegetated swales, buffers, and strips

# 5 Mixed-Use Development District Zone Designation

## Development Zones and Types of Uses

Each area that is rezoned to the Mixed-Use Development District is subject to being divided into three development zones as illustrated in figure 5.1. The map shows general locations for specific types of development in the NYS Route 250 area of the Mixed-Use Development District. There is some flexibility in the exact size, shape, and location of each zone.

The purpose of each zone is as follows:

### Zone A – High Density Core Mixed-Use

Areas identified as Zone A are intended to be most dense portions of the district with a mix of commercial/retail, office, civic, and compact residential uses. The percentage of residential is lowest compared to other uses within Zone A.

Permitted Uses: Emphasis is on vertical mixed-use, with commercial/retail/office on lower levels, and residential/office on upper levels.

### Zone B - Residential Mixed-Use

The majority of the district is designated as Zone B. These areas are intended to provide a mix of diverse residential uses.

Permitted Uses: Emphasis is on providing a variety of residential uses, with the addition of support commercial/retail/office.

### Zone C - Low Density Mixed-Use

Areas within Zone C are the least dense portions of the District. This area creates a buffer to the existing homes on Penfield Center Road. Uses within Zone C are intended to minimize the impact on existing natural resources and provide transition to adjacent land uses.

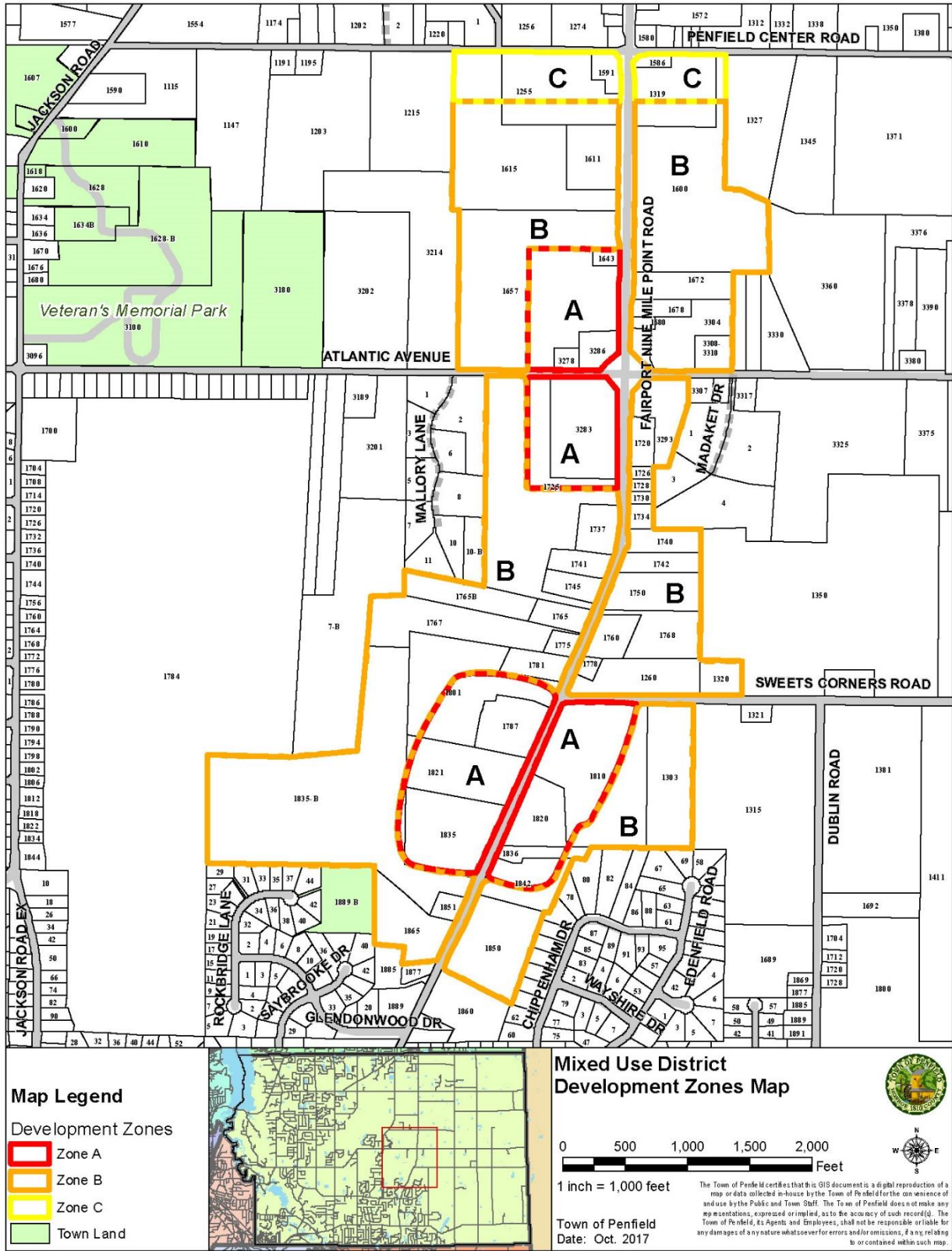


Figure 5.1. This map illustrates development zones for the Mixed-Use Development District.

# Appendix A: Acknowledgements

The Penfield Town Board recognizes the time and effort of the current and past staff, community members, property owners and others who provided comments and feedback that contributed to the updated Mixed-Use Development Manual

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James Costello, Town of Penfield  
Roseann Denoncourt, Planning Board/Small Business Owner  
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The NYS Route 250 Mixed-Use Development District Steering Committee:

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Bill Wickham, Wickham's Farm Market  
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Peg Salone, Country Club East Rep 1  
Kathi Milch, Country Club East Rep 2  
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Jack Odenbach, The Dolomite Group  
Dave Rinaldo, Monroe County Parks  
Jim Rowe, Ellison Heights HOA  
Andris Silins, Genesee Conservation League  
Terry Smith, Trails Committee  
Tim Hampton, Thomas Cove Resident  
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Arthur Woodward, Farm Brook/Panorama Trail Rep.

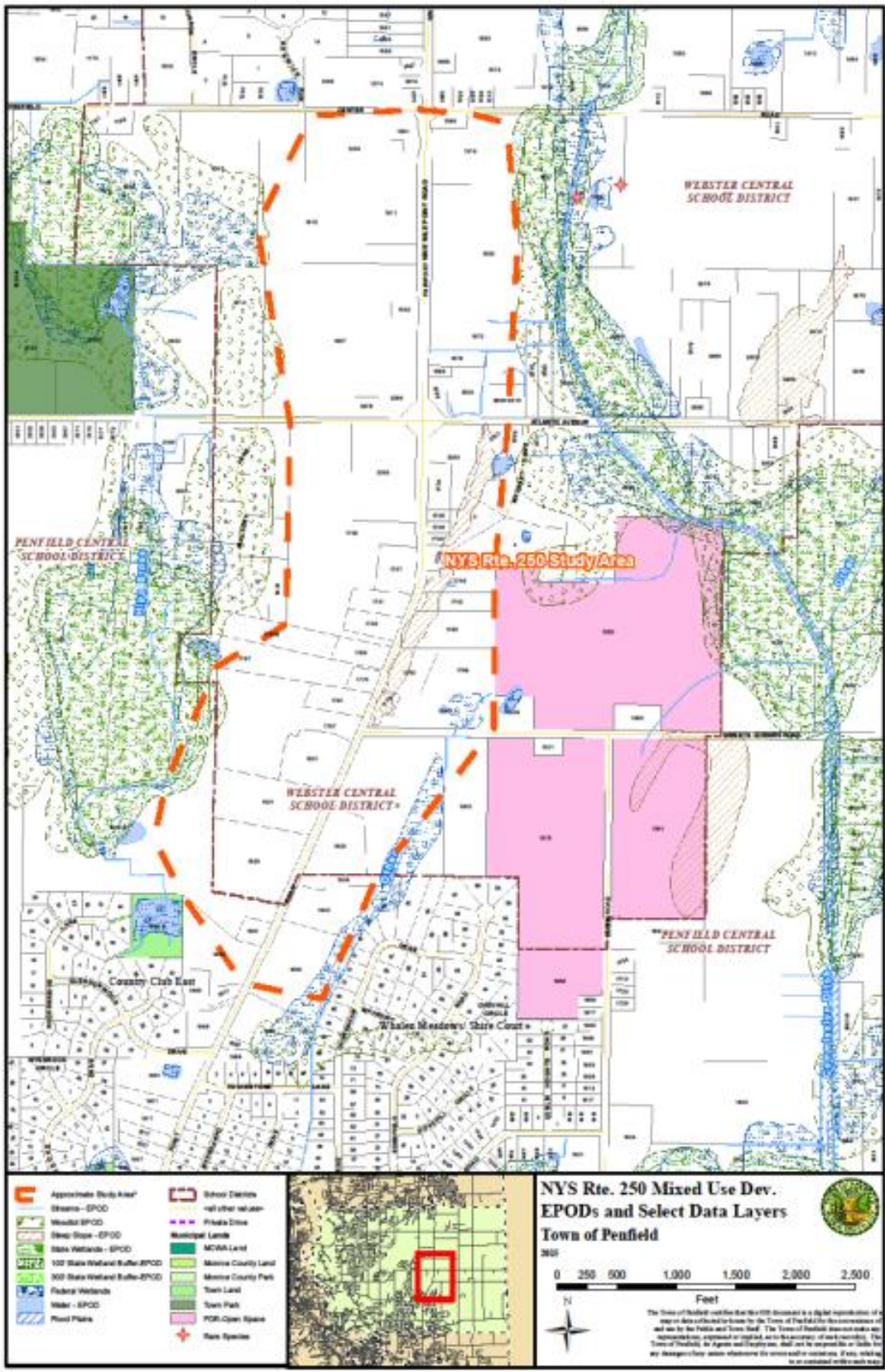
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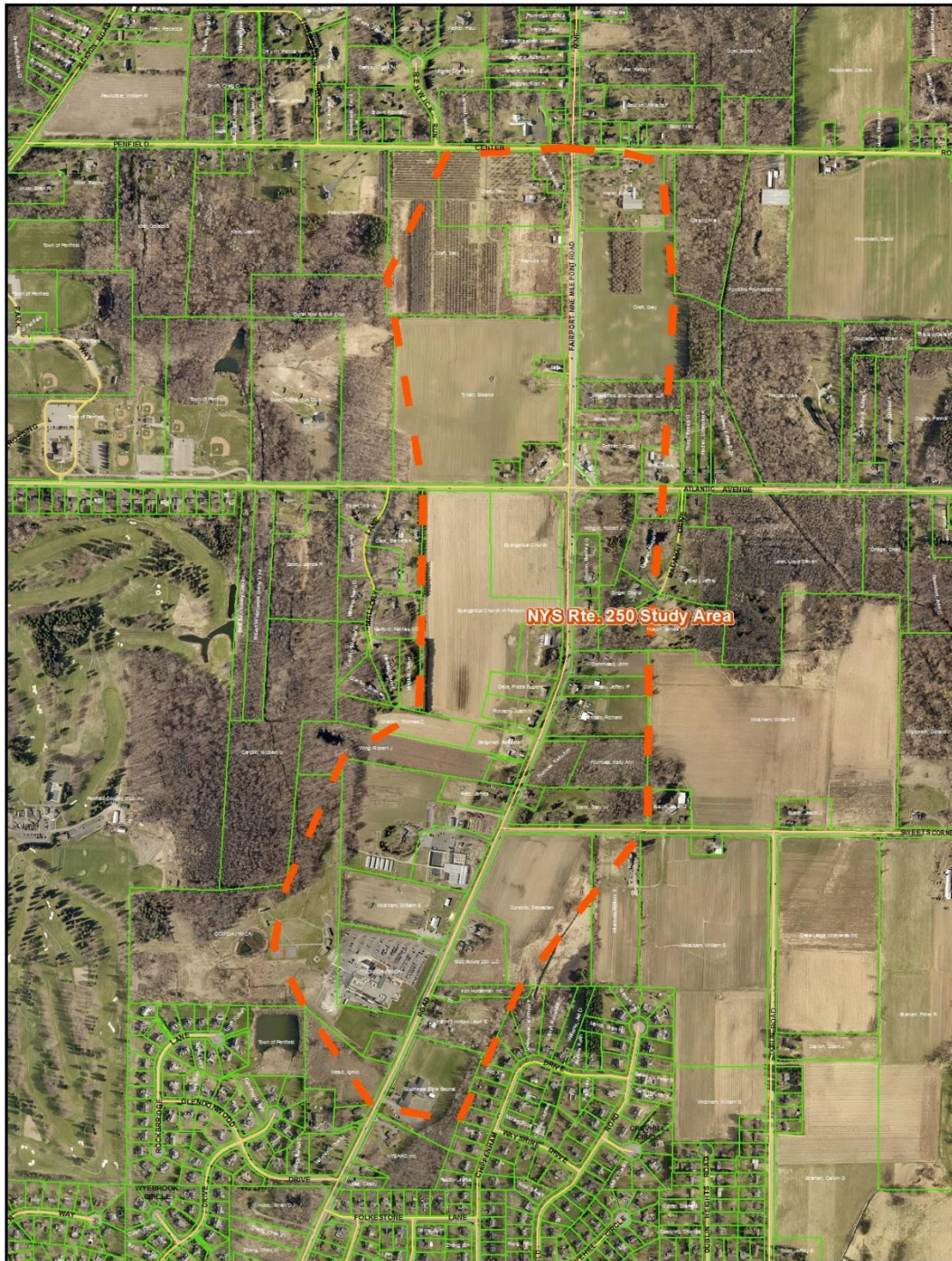
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Councilperson Rob Quinn

# Appendix B – Additional Mapping







**Approximate Study Area\***

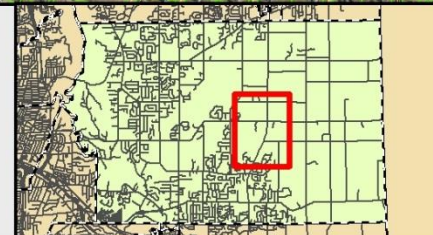
**Penfield Roads**

- Dedicated Roads
- Private Drive
- Parcels

**NYMONR017040.ecw**

**RGB**

- Red: Red
- Green: Green
- Blue: Blue



**NYS Rte. 250 Mixed Use Dev.**  
**Aerial with Parcel Owners**  
**Town of Penfield**  
 January, 2013

0 500 1,000 1,500 2,000 2,500  
 Feet

The Town of Penfield certifies that this GIS document is a digital reproduction of a map or data collected in-house by the Town of Penfield for the convenience of use by the Public and Town Staff. The Town of Penfield does not make any representations, expressed or implied, as to the accuracy of such record(s). The Town of Penfield, its Agents and Employees, shall not be responsible or liable for any damages of any nature whatsoever for errors and/or omissions, if any, relating to or contained within each map.

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# Appendix C: Examples of Mixed-Use Development

The following examples illustrate a wide range of possibilities and approaches to mixed-use. It should be recognized that, like Penfield's Mixed-Use Development District, each of these projects was designed and developed with specific district goals and objectives in mind. These projects are provided as examples, they are not necessarily compatible with Penfield's goals and objectives for mixed-use.

## The Market Common Clarendon

Arlington, Virginia

The Market Common Clarendon is an urban village that includes mixed-use retail, apartments, and Townhomes on a three-block, 10-acre site located in Arlington, Virginia. The development consists of 250,000 square feet of specialty retail, 300 apartments, and 87 Townhomes.

The mixed-use development is based on one- and two-story retail space in varying sizes and elevations consistent with the type of development along a community's Main Street. A new central boulevard includes an urban park in the median area. This park area is about a half-acre in size and contains green space, fountains, seating areas, a band shell, landscaping, and a small playground.

The east building is five stories tall with four apartment levels. The west building is six stories tall and is comprised of four levels of residential uses above two levels of retail. The Fillmore Street building includes four-story loft apartments along the street, and these step back 24 feet to a six-story apartment building that includes a six-level parking structure.



*Figures C.1 – C.2 The Village includes diverse housing types that are incorporated into a Village Center that includes residential and commercial uses.*

Lessons learned that can be applied to Penfield - Developers point to the emphasis on specialty retail and the density of the project as the reason for its success. These may not directly apply to Penfield, but the Market Commons program was appropriate for its given location.



**Figures C.3-C.5** The Market Commons Clarendon is an example of a traditional retail-orientated urban mixed-use development. Additional examples of housing in the Village Center.



## The Village at Niagara-on-the-Lake

Canada

The Village is an architecturally protected master planned community located in Niagara-on-the-Lake, Ontario. Painstakingly planned and carefully implemented, every aspect of The Village is intended to optimize the quality of life of its residents. The Village Centre offers a host of amenities within walking distance, and the unique home designs capture the authentic architecture of Niagara-on-the-Lake. Proudly returning to all the best values, principles, and traditions embodied in small Town Canada, The Village is a truly exceptional neighborhood.

Inspired by the inviting tree-lined streets of the Old Town Niagara-on-the-Lake, the streets in The Village are narrow with gentle curves and include sidewalks on both sides. This crucial combination has the wonderful result of calming traffic to a more civilized pace, providing a safe pedestrian environment. Upon completion, the five-acre village center will include a unique collection of shops, services, galleries, professional offices, cafes, and restaurants.

Lessons learned that can be applied to Penfield - The Village at Niagara illustrates that mixed-use can be successful in northern climates. Although relatively small, the village center has become a destination point and provides services for residents.

## Corn Hill Landing

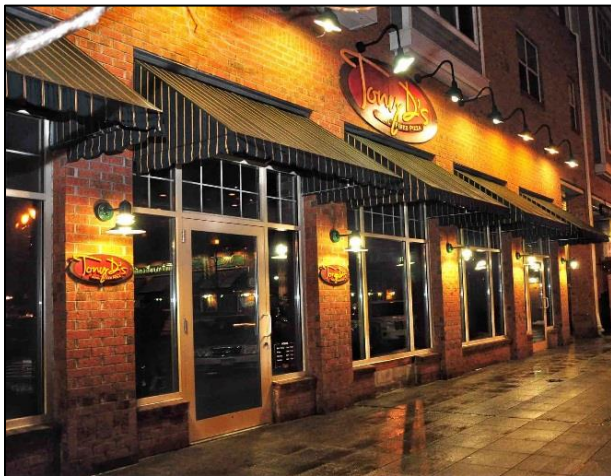
Rochester, NY

Corn Hill Landing is located in Rochester, NY along the Genesee River near the Douglass-Anthony Bridge. This mixed commercial/residential development includes 127 apartments, 15,000 square feet of office space, and 14,000 square feet of retail and restaurant space. Residents park in an underground garage and business customers use a small surface parking lot. The development includes a harbor master and access to boat docks. The Landing is across the street from Corn Hill, a restored Victorian residential neighborhood recognized as one of the finest in the country. Wrought iron fences, tall brick cupolas, and narrow, winding streets help define the neighborhood.



*Figure C.6. Corn Hill Landing is an example of a mixture of residential and commercial uses that can be successful in upstate New York.*

Lessons learned that can be applied to Penfield - Corn Hill Landing is one of the few mixed-use developments in the Rochester area. The quality of the architecture and the location of the development on the Genesee River are two reasons for its success. Limited parking has been an issue for the commercial uses.



*Figures C.7 - C.9. Corn Hill Landing is an example of a mixture of residential and commercial uses that can be successful in Rochester, New York.*

## Lantern Hill

Doylestown Borough, Pennsylvania

Lantern Hill is based upon concepts of a “traditional neighborhood development (TND)” that include an interconnected network of streets and sidewalks, building types based on the historic homes in the area, an emphasis on curb appeal where garages are accessible from alleys, public open space that encourage walkability and social interaction, and a mix of dwelling unit types reminiscent of the way older communities once developed. The development is based on Doylestown’s traditional-neighborhood-design building codes.

Lantern Hill TND includes 117 Townhome and single-family units, and 72,000 sq. ft. of office and retail units on 19 acres in Doylestown Borough, Pennsylvania. The builder's marketing brochure stated, "Inspired by the spirit and vitality of the familiar village, Lantern Hill replicates the great neighborhoods of the past, with a 21st century personality." The site was a former car-seat factory site at the intersection of Broad Street and Veterans Lane. It was the last large piece of land left in the borough.



*Figures C.10 - C.11. Lantern Hill TND includes a mix of detached and attached single-family homes.*

## Vintage Walk Buellton, CA

Vintage Walk, located on Avenue of the Flags in the City of Buellton (CA), is a mixed-use development built in 2007. The project marked the revitalization of this part of Buellton. The mixed-use development includes residential units that are intended to appeal to commuters from Santa Barbara who are looking to be closer to the South Coast. The development consists of 11 condominium residences and 10 office condominiums. There are also six affordable residences and one affordable office unit.

Lessons learned that can be applied to Penfield - Vintage Walk shows that even small-scale mixed-use developments can have a positive impact on revitalizing an area.



**Figure C.12.**  
*Vintage Walk integrates residential development with office space and public open space.*

## The Greene Town Center

Beavercreek, OH

The Greene Town Center is a mixed-use development in Dayton, OH. The Greene was built in two phases that covers 72 acres of land and costs approximately \$200 million. The developer provided the majority of the funding, but taxpayers provided \$14.8 million of the project, or eight percent of the total cost.

Phase I was completed in 2006 and it included tenants such as Lane Bryant, White House Black Market, Cheesecake Factory, Chico's, Sephora, Ann Taylor, and other retailers and restaurants. Phase I included 100,000 square feet of second-floor office space overlooking a Town square, and 136 residential loft apartments. Phase II was completed in the fall of 2008 and included other stores and shops to fill out the complex. Future development may include a comedy club, a fitness club, and an upscale movie theater.

The development hosts outdoor concerts and holiday events, and also includes a public space that is heavily used during all four seasons. In particular, children seem to enjoy playing in the main fountain during the warm summer months. Revenue from metered parking benefits a charitable foundation, while parking in lots and in garages is free.



Lessons learned that can be applied to Penfield - Although the Green Town Center is more retail oriented than Penfield is planning, the public open spaces provide the type of gathering spaces that promote social interaction.



**Figure C.13.** *The Green Town Center includes office space and apartments within a retail center.*

## Smyrna Market Village

Smyrna, Georgia

Smyrna is a small community located about 10 miles northwest of Atlanta, GA. It is known as the “Jonquil City,” a name derived from the thousands of jonquils that flourish in gardens and along the streets in early spring. About 10 years ago, the city decided to develop a new downtown from scratch, and the result is the Smyrna Market Village, a mixed-use development with civic uses as the anchor for residential and commercial development. The Smyrna Market Village consists of two major developments - the Village Green and the Market Village - both models of revitalization and redevelopment for other cities.

- Village Green - This is where Smyrna’s Community Center, Library, and arboretum reside. It is a focal point where people come together, and it is the central location of programmed outdoor activities and festivals. Adjacent to Village Green is private housing, retail and office space, Smyrna’s City Hall, and a public safety facility.



**Figure C.14.** *The streets in the Smyrna Market Village are occasionally closed for outdoor events and festivals.*



**Figure C.15.** *The streets in the Smyrna Market Village are occasionally closed for outdoor events and festivals.*

- Market Village - The Market Village is the heart of Smyrna. The Village consists of Smyrna’s Main Street with development on both sides to create a central axis leading to City Hall. The Village features 16 Townhomes over the top of first-floor retail space, over 40,000 square feet of retail space, 18,000 square feet of office space, and seven restaurants. The tallest structures are three stories in height, so the entire development has a pedestrian feel.

Lessons learned that can be applied to Penfield - The Smyrna Market Village is an example of a civic-oriented, mixed-use development that incorporates residential, commercial, and service uses into one cohesive development. The Village has become a landmark for Smyrna.

## Northwest Crossing

Bend, Oregon

This 486-acre mixed-use community includes parks, locally owned businesses, and residential units. The houses are certified by Oregon’s Earth Advantage Program and the development employs sustainable design principles derived from some of Bend’s older neighborhoods. These design principles include a grid of interconnected narrow streets, a mix of architectural styles, shops, parks, and schools within walking distance of most homes. These design principles have resulted in safe, attractive, walkable tree-lined streets and neighborhoods that encourage social interaction. The community has proven to be a compelling marketing tool, attracting a range of residents from young couples to retirees.



**Figures C.16-C.17.** *Northwest Crossing is a full-service development that provides a wide range of housing opportunities.*

Northwest Crossing was designed to include a wide range of lots, housing types, and sizes. Housing options include single-family homes, duplexes, accessory dwellings, Townhomes and condominiums, multifamily apartments, cottages, and apartments located above commercial/retail uses. The basic characteristics of the development are as follows:

- 486 acres total at buildout
- Average density: 5.2 du/acre (through present development)
- 1,350 homes at buildout
- 26.4 acres of neighborhood and community parks plus 6.8 acres of natural open space
- 63 acres of school property
- 36 acres of mixed-use and commercial development

Lessons learned that can be applied to Penfield - Northwest Crossing is a comprehensive, average density development that is similar to the types of neighborhoods that may be appropriate for Penfield. It includes high quality architecture and an integration of structures, landscape, and site development that would work well in Penfield.

## Market Common

Myrtle Beach, South Carolina

Market Common is an upscale shopping and live/work community located on the former Myrtle Beach Air Force Base. It has become one of the most desired areas for living along the Grand Strand, the large stretch of beaches extending from Little River to Georgetown, SC. New homes are the majority of real estate selling in the Market Common District, and many single-family and Townhome communities are available. The housing environment of the Market Common area is a part of the thriving Myrtle Beach real estate market.

The shops of Market Common opened in 2008. They include a number of popular shops and restaurants. The development includes parks, walkways, and other public amenities. Adjacent to the Market Common are sport fields, the Virginia Clinic and Horry Georgetown Technical College.

Lessons Learned that can be applied to Penfield - Market Common may be appropriate when redeveloping the more commercial developments in Penfield.



**Figures C.18 - C.20.** *The Market Common transformed a former air force base into a mixed-use development.*

## Blue Black Square Mixed-Use Development

West Hartford, Connecticut

The Blue Black Square Mixed-Use Development consists of eight buildings on 40 acres that incorporates the Blue Black Square, Webster Walk, Noah Webster Library, police station, and Town hall. The sitework includes updated hardscape and lighting. The one million square feet of interior space includes condominiums, parking, retail and commercial space, and public works.

Along with retail stores, the Blue Black Square community includes:

- The Lofts at Blue Black Square: 48 contemporary loft-style rental apartments
- The Heritage: 59 luxury condominiums
- The Lexicon: 60,000-square foot, Class A professional office building
- The Rutherford: 140,000-square foot professional and medical office building anchored by Hartford Hospital Outpatient Surgery Center

Lessons learned that can be applied to Penfield - Blue Black Square is much denser than what is being discussed for Penfield, but it shows an example of creating high end mixed-use with an emphasis on office use.



**Figure C.21.** The Blue Black Square is predominantly an office, professional, and medical facility that includes condominiums and lofts.

## Third Street Cottages

Langley, Washington

The Third Street Cottages is a small residential development that demonstrated eight smaller cottages could work on a site originally planned for four larger homes. The development was a response to the City of Langley's (WA) innovative "Cottage Housing Development" (CHD) zoning code. The purpose of this provision was to preserve housing diversity, affordability, and character, and to discourage the spread of sprawl. The code allows for up to double the density of detached homes in all single-family zones providing that the ground floor area is less than 700 square feet. The cottages must also face a usable landscaped commons. Each project proposed is reviewed by the planning and design review boards.

The community of eight detached cottages is located on four standard single-family lots (31,000 SF total area). The homes are approximately 650 square feet, with lofts up to 200 square feet, and are conveyed as condominium ownership. They are situated around a shared garden with a commons building and toolshed. Parking is detached and open, and every residence has a storage room. To ensure privacy between cottages, the houses 'nest' together, with the 'open' side of one house facing the 'closed' side of the next. A pea-patch garden is on one end of the Commons. The Tool Shed provides a spot for shared garden tools.

Each cottage, though similar, is unique. The cottages are painted with a combination of 24 different colors. Each household creates their own garden landscape and flowerbox garden. Another way the cottages are unique is because homeowners name their own cottage.

Lessons learned that can be applied to Penfield - This type of cottage development provides another option for housing that is needed in Penfield, and it can be accomplished in a very small space.



*Figure C.22. The Third Street Cottages are centered around a central public space that encourages social interaction.*

## Ericksen Cottages

Bainbridge Island, Washington

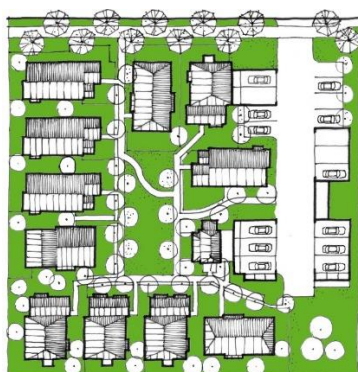
The Ericksen Cottages are located on Bainbridge Island, a short three-minute ferry ride west of downtown Seattle, WA. The development consists of 11 two-bedroom and three-bedroom cottages clustered around a public courtyard. Emphasis was on creating a compact, connected, walkable community that would provide an alternative, affordable source of housing for small households. The cottages have been extremely popular because they provide a quiet, peaceful retreat in Winslow, the small Town that serves as the point of departure for ferry travelers. Winslow is a small, walkable community that has an abundance of small, quaint restaurants, shops, and night spots.

Each cottage includes its own private yards surrounded by a low fence. The central courtyard is actually a quiet, relaxing garden that serves as a public gathering space. All cottages include a room-sized covered porch, front porch flower boxes, private yards, freestanding gas fireplaces, regional hardwood floors and cabinets, bay windows, vaulted ceilings, wood paneling, and classic details.

Lessons learned that can be applied to Penfield - As with Third Street Cottages, this development helps create a walkable community in a small space. This approach would provide for more diverse housing in Penfield.



**Figures C.23 - C.25.** The Ericksen Cottages are within walking distance of the Bainbridge Ferry, which takes commuters and tourists to downtown Seattle.



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